

# **AGENDA ITEM**

**2.a.**

CACHE COUNTY GOVERNMENT  
REVENUES WITH COMPARISON TO BUDGET  
FOR THE 2 MONTHS ENDING FEBRUARY 28, 2025

AIRPORT FUND

		ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEARNED	PCNT
	<u>INTERGOVERNMENTAL REVENUE</u>						
277-33-70105	LOGAN CITY-SHARED NET EXP	.00	.00	.00	100,000.00	100,000.00	.0
	TOTAL INTERGOVERNMENTAL REVE	.00	.00	.00	100,000.00	100,000.00	.0
	<u>MISCELLANEOUS REVENUE</u>						
277-36-10000	INTEREST	.00	.00	.00	30,000.00	30,000.00	.0
277-36-15000	GAS TAX REFUND	.00	2,011.63	2,011.63	8,000.00	5,988.37	25.2
277-36-16000	LANDING FEES	.00	.00	.00	5,500.00	5,500.00	.0
277-36-18000	FUEL FLOW -STORAGE FEES	.00	.00	.00	20,000.00	20,000.00	.0
277-36-19000	FIRE DEPARTMENT STANDBY FEES	.00	.00	.00	5,000.00	5,000.00	.0
277-36-90000	SUNDRY REVENUE	.00	500.00	500.00	4,000.00	3,500.00	12.5
	TOTAL MISCELLANEOUS REVENUE	.00	2,511.63	2,511.63	72,500.00	69,988.37	3.5
	<u>AIRPORT LAND LEASE REVENUES</u>						
277-37-80000	AIRPORT FEES-LAND LEASE INCOM	.00	.00	.00	113,400.00	113,400.00	.0
	TOTAL AIRPORT LAND LEASE REVE	.00	.00	.00	113,400.00	113,400.00	.0
	<u>CONTRIBUTIONS &amp; TRANSFERS</u>						
277-38-20000	CONTRIBUTION - CACHE COUNTY	.00	.00	.00	100,000.00	100,000.00	.0
277-38-90000	APPROPRIATED FUND BALANCE	.00	.00	.00	629,022.00	629,022.00	.0
	TOTAL CONTRIBUTIONS & TRANSFE	.00	.00	.00	729,022.00	729,022.00	.0
	TOTAL FUND REVENUE	.00	2,511.63	2,511.63	1,014,922.00	1,012,410.37	.3

CACHE COUNTY GOVERNMENT  
EXPENDITURES WITH COMPARISON TO BUDGET  
FOR THE 2 MONTHS ENDING FEBRUARY 28, 2025

AIRPORT FUND

	ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEXPENDED	PCNT
<u>AIRPORT</u>						
277-4460-110 FULL TIME EMPLOYEES	.00	17,201.08	17,201.08	162,673.00	145,471.92	10.6
277-4460-115 OVERTIME	.00	44.19	44.19	3,000.00	2,955.81	1.5
277-4460-120 PART TIME EMPLOYEES	.00	4,963.75	4,963.75	62,428.00	57,464.25	8.0
277-4460-130 EMPLOYEE BENEFITS	.00	10,582.78	10,582.78	114,521.00	103,938.22	9.2
277-4460-210 SUBSCRIPTIONS & MEMBERSHIPS	.00	.00	.00	100.00	100.00	.0
277-4460-220 PUBLIC NOTICES	.00	.00	.00	300.00	300.00	.0
277-4460-230 TRAVEL	.00	1,294.04	1,294.04	4,000.00	2,705.96	32.4
277-4460-240 OFFICE SUPPLIES	.00	7.49	7.49	1,500.00	1,492.51	.5
277-4460-250 EQUIPMENT SUPPLIES & MAINT	2,675.98	1,593.35	4,269.33	46,000.00	41,730.67	9.3
277-4460-251 NON CAPITALIZED EQUIPMENT	.00	.00	.00	13,000.00	13,000.00	.0
277-4460-260 BUILDING & GROUNDS	.00	3,387.99	3,387.99	25,000.00	21,612.01	13.6
277-4460-261 SNOW REMOVAL	.00	.00	.00	65,000.00	65,000.00	.0
277-4460-262 VEGETATION CONTROL - CHEMICAL	.00	.00	.00	12,000.00	12,000.00	.0
277-4460-263 VEGETATION CONTROL - MOWING	.00	.00	.00	5,000.00	5,000.00	.0
277-4460-270 UTILITIES	.00	3,605.24	3,605.24	27,000.00	23,394.76	13.4
277-4460-280 COMMUNICATIONS	.00	571.37	571.37	5,000.00	4,428.63	11.4
277-4460-290 FUEL	.00	1,371.68	1,371.68	10,000.00	8,628.32	13.7
277-4460-291 UNION PACIFIC PROPERTY LEASE	.00	.00	.00	20,000.00	20,000.00	.0
277-4460-311 PROFESSIONAL SERVICES	.00	.00	.00	1,000.00	1,000.00	.0
277-4460-330 EDUCATION & TRAINING	.00	275.00	275.00	44,000.00	43,725.00	.6
277-4460-510 INSURANCE	.00	.00	.00	18,000.00	18,000.00	.0
277-4460-621 MISC BOARD SERVICES/TRAVEL	.00	1,470.53	1,470.53	8,000.00	6,529.47	18.4
277-4460-625 LOGAN FIRE - STANDBY FEES	.00	.00	.00	8,000.00	8,000.00	.0
277-4460-739 GRANT PROJECTS	43,249.00	.00	43,249.00	.00	( 43,249.00)	.0
277-4460-740 CAPITALIZED EQUIPMENT	9,999.50	.00	9,999.50	.00	( 9,999.50)	.0
TOTAL AIRPORT	55,924.48	46,368.49	102,292.97	655,522.00	553,229.03	15.6

CACHE COUNTY GOVERNMENT  
EXPENDITURES WITH COMPARISON TO BUDGET  
FOR THE 2 MONTHS ENDING FEBRUARY 28, 2025

AIRPORT FUND

		ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEXPENDED	PCNT
<u>CONTRIBUTIONS</u>							
277-4800-477	TRANSFER OUT - AIRPORT CAPITAL	.00	.00	.00	359,400.00	359,400.00	.0
	TOTAL CONTRIBUTIONS	.00	.00	.00	359,400.00	359,400.00	.0
	TOTAL FUND EXPENDITURES	55,924.48	46,368.49	102,292.97	1,014,922.00	912,629.03	10.1
	NET REVENUE OVER EXPENDITURES	( 55,924.48)	( 43,856.86)	( 99,781.34)	.00	99,781.34	.0

CACHE COUNTY GOVERNMENT  
REVENUES WITH COMPARISON TO BUDGET  
FOR THE 2 MONTHS ENDING FEBRUARY 28, 2025

CAPITAL PROJECT AIRPORT

		ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEARNED	PCNT
477-33-15000	FED GRANT - SCASDP	.00	.00	.00	1,203,000.00	1,203,000.00	.0
477-33-44402	STATE GRANT	.00	.00	.00	862,200.00	862,200.00	.0
	TOTAL SOURCE 33	.00	.00	.00	2,065,200.00	2,065,200.00	.0
	<u>SOURCE 38</u>						
477-38-10277	TRANSFER IN - AIRPORT	.00	.00	.00	359,400.00	359,400.00	.0
	TOTAL SOURCE 38	.00	.00	.00	359,400.00	359,400.00	.0
	TOTAL FUND REVENUE	.00	.00	.00	2,424,600.00	2,424,600.00	.0

CACHE COUNTY GOVERNMENT  
EXPENDITURES WITH COMPARISON TO BUDGET  
FOR THE 2 MONTHS ENDING FEBRUARY 28, 2025

CAPITAL PROJECT AIRPORT

	ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEXPENDED	PCNT
<u>AIRPORT</u>						
477-4460-730 IMPROVEMENTS	.00	.00	.00	2,222,600.00	2,222,600.00	.0
477-4460-740 CAPITALIZED EQUIPMENT	.00	.00	.00	202,000.00	202,000.00	.0
TOTAL AIRPORT	.00	.00	.00	2,424,600.00	2,424,600.00	.0
TOTAL FUND EXPENDITURES	.00	.00	.00	2,424,600.00	2,424,600.00	.0
NET REVENUE OVER EXPENDITURES	.00	.00	.00	.00	.00	.0

**LOGAN - CACHE AIRPORT AUTHORITY BOARD MEETING**  
**FEBRUARY 6, 2025**  
**DRAFT MINUTES**

The Logan-Cache Airport Authority Board convened in a regular session on Thursday February 6, 2025 at 8:30 a.m. in the Cache County Historic Courthouse, County Council Chambers, 199 North Main, Logan, Utah.

**ATTENDANCE**

**Members of the Airport Authority Board in Attendance:**

David Zook – Chair -- Cache County Executive  
John Kerr – Vice Chair – At large appointment by Airport Authority Board  
Mayor Holly Daines – Logan City Mayor  
Jeannie F. Simmonds – Logan City Council – At large appointment by Logan City  
Brett Hugie – At large appointment by Logan City  
Ryan Snow – At large appointment by Cache County  
Kirk Jensen – Logan City Economic Development Director – Ex-officio Advisory Member  
Shawn Milne – Director of Regional Economic Development – Ex-officio Advisory Member

**Members of the Airport Authority Board Absent:**

Keegan Garrity – Cache County Council – At large appointment by Cache County

**Also in Attendance:**

Bob Low – Airport Manager  
Kasey Maxwell – Airport  
Valerie Black – Airport  
Wesley Bingham – Cache County Finance Director  
Robert Stephens – Logan City Fire Department  
Conner Butterfield – Lochner Engineering  
Judd Hill – Lochner Engineering  
Antony Congram – Lochner Engineering  
Aaron Dyches – USU Aviation  
Baron Wesemann – USU Aviation  
Scott Weaver – Leading Edge Aviation  
Kim Hall  
Dan Boston  
Marv Halling  
Robert Kidd  
Janeen Allen – Minutes

**1. CALL TO ORDER**

Chairman David Zook called the meeting to order at 8:30 a.m. and welcomed new board member, Keegan Garrity, who replaced Karl Ward on the Cache County Council. Garrity was unable to

attend the meeting but will be in attendance at the next board meeting. Zook also welcomed Brett Hugie, who was recently reappointed by Logan City. He expressed appreciation to Hugie for his willingness to continue to serve on the board.

## **2. APPROVAL OF MINUTES**

### **a. APPROVAL OF MINUTES – January 2, 2025**

**ACTION: Motion was made by Brett Hugie and seconded by Ryan Snow to approve the minutes of January 2, 2025 as amended. The vote in favor was unanimous, 6-0 (Keegan Garrity absent for vote)**

## **3. MANAGER REPORT**

### **a. TITLE VI PROGRAM**

Bob Low introduced airport intern, Valerie Black, who reported on the Title VI program which assures non-discrimination under US Department of Transportation funding. She explained the training and complaint handling procedures at the airport.

Mayor Daines inquired about enforcement of Title VI under the current federal administration.

Judd Hill of Lochner Engineering advised to proceed cautiously and wait for guidance from the FAA on the program.

### **b. AIRPORT BUDGET (ATTACHMENT A)**

Mr. Low presented the 2025 budget and explained the sources of revenue, including Logan City and Cache County contributions, interest, gas refunds, and landing fees.

Jeannie Simmonds asked about the contribution amounts from the county and city.

Wesley Bingham, County Finance Director, discussed the timing of contributions and the need for further discussion.

Mr. Low provided a detailed breakdown of the budget, including expenses for full-time employees, part-time employees, the internship program, and maintenance costs.

As part of the budget discussion, Mr. Low talked about the relationship with County Public Works for snow removal and the decision to call them even for small snow events.

Board members discussed the possibility of purchasing a used snow removal truck from Salt Lake International Airport.

After Mr. Low explained current snow removal practices and the need for better equipment, the Board agreed to continue with the current practices while considering future equipment purchases.

#### **4. ACTION AND DISCUSSION ITEMS**

a. **UPDATE AND CONSIDERATION OF AIRPORT LANDING FEES – ANNIE TEIXEIRA, VECTOR AIRPORT SYSTEMS**

Mr. Low introduced Annie Teixeira of Vector Flight Services to discuss corporate landing fees. She was attending the meeting via Google Meet and presented revenue numbers and scenarios for implementing landing fees, including a minimum weight of 6,000 pounds. She explained the process for implementing landing fees, including communication with operators and the timeline for billing.

Board members discussed the impact of landing fees on traffic and the need for fair and equitable fees.

b. **CONSIDERATION AND APPROVAL OF AMENDED LAND LEASE AGREEMENT – BOB LOW**

Chairman Zook referred to the redlined land lease agreement included in the meeting information packet noting that feedback from hangar owners and board members was taken into consideration in the draft version.

Board members discussed the changes, including the rate for hangar footprints and the responsibility for maintenance.

Ryan Snow suggested encouraging future tenants to use zero lot line construction to maximize space.

**ACTION: Motion was made by Mayor Holly Daines and seconded by John Kerr to approve the lease agreement with the amendments discussed in the meeting. The vote in favor was unanimous, 6-0 (Keegan Garrity absent for vote)**

c. **CONSIDERATION OF USU CONTRIBUTION AND PARTICIPATION – BRETT HUGIE**

Brett Hugie reported on the discussions he and Mayor Daines and Chairman Zook have had with USU, particularly Dave Cowley, regarding Part 139 expenses and reimbursements. The university agreed to reimburse last year's expenses and contribute to future annual expenses.

The Board discussed the possibility of adding a seat for a representative from the university and agreed to continue consideration in future meetings.

d. **UPDATE ON FBO FACILITY EXPANSION – SCOTT WEAVER, LEADING EDGE AVIATION**

Scott Weaver of Leading Edge Aviation provided an update on the FBO facility expansion, including the need for a lease and preliminary financing.

The Board discussed the current status of the project and the potential impact of interest rates. Mr. Weaver noted that the fuel farm has been pre-approved for financing.

Board members agreed to continue working on the lease and financing aspects of the expansion project.

Mr. Hugie noted that the university supports Mr. Weaver's proposal for USU teams.

**e. REQUEST FOR RECONSIDERATION OF INSTALLATION OF SELF-FUELING STORAGE TANK SCOTT WEAVER, LEADING EDGE AVIATION – SCOTT WEAVER, LEADING EDGE AVIATION**

Mr. Weaver was in the process of acquiring architectural drawings for a smaller tank, but Mr. Kerr asked them to hold to see if there were any other funding sources available. Since no funding sources have been found, Mr. Weaver asked the Board to continue pursuing architectural and engineering drawings for a 15,000-gallon self-serve gas tank.

Mayor Daines asked if the proposal is in line with the master plan. This was confirmed by Judd Hill of Lochner Engineering.

Mr. Kerr expressed support for the proposal citing funding as the main issue.

Mr. Snow asked about the cost of the project which is estimated at around half a million.

Questions were raised about the cost versus the potential revenue from selling the fuel.

Mr. Weaver said that the rising cost of labor and fuel, and the need to be competitive with surrounding airports makes the project necessary.

Mr. Snow proposed a public/private partnership to share the cost and revenue.

After more discussion and questions about funding, Chairman Zook asked Mr. Weaver to continue working with Mr. Low and others and give an update report at the next meeting.

**f. MASTER PLAN STATUS UPDATE – JUDD HILL, LOCHNER ENGINEERING**

Judd Hill introduced Antony Congram of Lochner Engineering who will be working with the Logan-Cache Airport as a design engineer out of the Lochner Salt Lake office.

Mr. Hill said the master plan has now moved through the Technical Advisory Committee and the final step is to hold a public open house to answer any questions from the public about the airport layout and master plan. They are looking at the last week of March to hold it potentially at the airport. It was suggested that they hold the open house on Saturday, April 12<sup>th</sup>, when they have a flyover in order to attract more people.

After the Open House, the plan will then be sent to the FAA for their final review.

**g. TOWER REPAIR UPDATE – BOB LOW**

Mr. Low introduced Kasey Maxwell, who recently was hired full time, and had him review the bids. Mr. Maxwell presented three bids:

- Raymond Construction: \$90,465 – Demolition: \$70,000
- Sorensen and Gnehm Construction: \$107,000
- Paul Davis Construction: \$32,000

Mayor Daines questioned the cost differences and the materials used. She indicated that she still leans towards demolition of the tower due to future expansion plans.

Mr. Snow explained that Paul Davis responded specifically to the specifications and has a state contract. He suggested using the UDOT grant to cover the cost and adding a mural for historical preservation with the remaining grant funds.

Mr. Zook suggested verifying the legitimacy of the glass replacement in the Paul Davis Construction bid and that it will meet code.

**ACTION: Motion was made by Ryan Snow and seconded by John Kerr to award the contract to Paul Davis Construction in the amount of \$32,132.50 with the condition that the staff verify the issues discussed related to the glass.**

**Motion passes:**

**Aye – 5 votes: Hugie, Snow, Kerr, Zook, Simmonds,**

**Nay – 1 vote: Daines**

**(Keegan Garrity absent for vote)**

**h. OPEN ITEMS**

- Professor Wesemann reminded the board about the upcoming Air Fest on June 28<sup>th</sup>. The event will feature various performers and will be an open house with free admission.

**5. NEXT SCHEDULED BOARD MEETING**

Board members discussed the next board meeting due to the conflict with the UAOA Spring Conference being held the same time as the regularly scheduled meeting. It was decided to move the meeting one week later to March 13, 2025.

**6. ADJOURNMENT**

The meeting adjourned at 10:25 a.m.

LOGAN – CACHE AIRPORT AUTHORITY BOARD  
FEBRUARY 6, 2025

# ATTACHMENT A

# FAA CIVIL RIGHTS – TITLE VI

## FAA CONTRACT PROVISIONS



**Disclaimer:** The purpose of this document is to assist Airports with their compliance efforts, it does not alter or modify the terms of any applicable statute or regulation, is not a substitute for reading the regulation or the FAA Contract Provision Guidelines, nor does it constitute legal advice.

Federal laws and regulations require that an Airport include specific clauses in certain contracts regardless of whether or not the project is federally funded.

The term “contractor” is understood to mean a contractor or subcontractor and means one who participates through a contract or subcontract (at any tier).

All text provided in each listed provision is noted as “MANDATORY TEXT,” which means it must be used without modification. Please see the guidelines in the FAA link below for further information.

**FAA Contract Provision Guidelines: Federal Certifications**

<https://www.faa.gov/sites/faa.gov/files/2023-01/combined-federal-contract-provisions-2023-1-20.pdf>

## Civil Rights - Title VI - FAA Contract Provisions

### CIVIL RIGHTS

*The Airport must include this provision in all contracts, subcontracts, lease agreements or transfer agreements. MANDATORY TEXT*

#### Civil Rights General

In all its activities within the scope of its airport program, the Contractor agrees to comply with pertinent statutes, Executive Orders, and such rules as identified in Title VI List of Pertinent Nondiscrimination Acts and Authorities to ensure that no person shall, on the grounds of race, color, national origin (including limited English proficiency), creed, sex (including sexual orientation and gender identity), age, or disability be excluded from participating in any activity conducted with or benefiting from Federal assistance. This provision is in addition to that required by Title VI of the Civil Rights Act of 1964.

*Included the additional provision with the above without modification; the applicable text is based on whether the contract is a general contract or whether the contract is a lease or transfer agreement.*

*For General Contract Agreements contracts that do not involve property agreements.*

The above provision binds the Contractor and subcontractors from the bid solicitation period through the completion of the contract.

*For Lease Agreements & Transfer Agreements; property agreements such leases of concession space in a terminal and leases where a physical portion of the airport is transferred for use.*

If the Contractor transfers its obligation to another, the transferee is obligated in the same manner as the Contractor. The above provision obligates the Contractor for the period during which the property is owned, used or possessed by the Contractor and the airport remains obligated to the Federal Aviation Administration.

### CIVIL RIGHTS - TITLE VI ASSURANCES

*The Airport must include this provision in all solicitations for bids, requests for proposals, any work, or any negotiated agreements. MANDATORY TEXT*

#### Title VI Solicitation Notice:

Roswell Air Center, in accordance with the provisions of Title VI of the Civil Rights Act of 1964 (78 Stat. 252, 42 USC §§ 2000d to 2000d-4) and the Regulations, hereby notifies all bidders or offerors that it will affirmatively ensure that for any contract entered into pursuant to this advertisement, [select businesses, or disadvantaged business enterprises or airport concession disadvantaged business enterprises] will be afforded full and fair opportunity to submit bids in response to this invitation and no businesses will be discriminated against on the grounds of race, color, national origin (including limited English proficiency), creed, sex (including sexual orientation and gender identity), age, or disability in consideration for an award.

*The Airport must include this provision in all contracts, subcontracts, or agreements.*  
**MANDATORY TEXT**

#### Title VI List of Pertinent Nondiscrimination Acts and Authorities

During the performance of this contract, the Contractor, for itself, its assignees, and successors in interest (hereinafter referred to as the “Contractor”) agrees to comply with the following non-discrimination statutes and authorities; including but not limited to:

- Title VI of the Civil Rights Act of 1964 (42 USC § 2000d et seq., 78 stat. 252) (prohibits discrimination on the basis of race, color, national origin);
- 49 CFR part 21 (Non-discrimination in Federally-Assisted programs of the Department of Transportation—Effectuation of Title VI of the Civil Rights Act of 1964);
- The Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, (42 USC§4601) (prohibits unfair treatment of persons displaced or whose property has been acquired because of Federal or Federal-aid programs and projects);
- Section 504 of the Rehabilitation Act of 1973 (29 USC § 794 et seq.), as amended (prohibits discrimination on the basis of disability); and 49 CFR part 27 (Nondiscrimination on the Basis of Disability in Programs or Activities Receiving Federal Financial Assistance);
- The Age Discrimination Act of 1975, as amended (42 USC § 6101 et seq.) (prohibits discrimination on the basis of age);
- Airport and Airway Improvement Act of 1982 (49 USC § 47123), as amended (prohibits discrimination based on race, creed, color, national origin, or sex);
- The Civil Rights Restoration Act of 1987 (PL 100-259) (broadened the scope, coverage and applicability of Title VI of the Civil Rights Act of 1964, the Age Discrimination Act of 1975 and Section 504 of the Rehabilitation Act of 1973, by expanding the definition of the terms “programs or activities” to include all of the programs or activities of the Federal-aid recipients, sub-recipients and contractors, whether such programs or activities are Federally funded or not);
- Titles II and III of the Americans with Disabilities Act of 1990 (42 USC § 12101, et seq) (prohibit discrimination on the basis of disability in the operation of public entities, public and private transportation systems, places of public accommodation, and certain testing entities) as implemented by U.S. Department of Transportation regulations at 49 CFR parts 37 and 38;
- The Federal Aviation Administration’s Nondiscrimination statute (49 USC § 47123) (prohibits discrimination on the basis of race, color, national origin, and sex);
- Executive Order 12898, Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations (ensures nondiscrimination against minority populations by discouraging programs, policies, and activities with disproportionately high and adverse human health or environmental effects on minority and low-income populations);
- Executive Order 13166, Improving Access to Services for Persons with Limited English Proficiency, and resulting agency guidance, national origin discrimination includes discrimination because of limited English proficiency (LEP). To ensure compliance with Title VI, you must take reasonable steps to ensure that LEP persons have meaningful access to your programs [70Fed. Reg. 74087 (2005)];

- Title IX of the Education Amendments of 1972, as amended, which prohibits you from discriminating because of sex in education programs or activities (20 USC § 1681, et seq).

*The Airport must include this provision in all contracts, subcontracts, or service contracts with utility companies.*

MANDATORY TEXT

#### Nondiscrimination Requirements/ Title VI Clauses for Compliance

##### Compliance with Nondiscrimination Requirements:

During the performance of this contract, the Contractor, for itself, its assignees, and successors in interest (hereinafter referred to as the "Contractor"), agrees as follows:

1. Compliance with Regulations: The Contractor (hereinafter includes consultants) will comply with the Title VI List of Pertinent Nondiscrimination Acts and Authorities, as they may be amended from time to time, which are herein incorporated by reference and made a part of this contract.
2. Nondiscrimination: The Contractor, with regard to the work performed by it during the contract, will not discriminate on the grounds of race, color, national origin (including limited English proficiency), creed, sex (including sexual orientation and gender identity), age, or disability in the selection and retention of subcontractors, including procurements of materials and leases of equipment. The Contractor will not participate directly or indirectly in the discrimination prohibited by the Nondiscrimination Acts and Authorities, including employment practices when the contract covers any activity, project, or program set forth in Appendix B of 49 CFR part 21.
3. Solicitations for Subcontracts, including Procurements of Materials and Equipment: In all solicitations, either by competitive bidding or negotiation made by the Contractor for work to be performed under a subcontract, including procurements of materials, or leases of equipment, each potential subcontractor or supplier will be notified by the Contractor of the contractor's obligations under this contract and the Nondiscrimination Acts and Authorities on the grounds of race, color, or national origin.
4. Information and Reports: The Contractor will provide all information and reports required by the Acts, the Regulations, and directives issued pursuant thereto and will permit access to its books, records, accounts, other sources of information, and its facilities as may be determined by the Sponsor or the Federal Aviation Administration to be pertinent to ascertain compliance with such Nondiscrimination Acts and Authorities and instructions. Where any information required of a contractor is in the exclusive possession of another who fails or refuses to furnish the information, the Contractor will so certify to the Sponsor or the Federal Aviation Administration, as appropriate, and will set forth what efforts it has made to obtain the information.
5. Sanctions for Noncompliance: In the event of a Contractor's noncompliance with the non-discrimination provisions of this contract, the Sponsor will impose such contract sanctions as it or the Federal Aviation Administration may determine to be appropriate, including, but not limited to:
  - a. Withholding payments to the Contractor under the contract until the Contractor complies; and/or
  - b. Cancelling, terminating, or suspending a contract, in whole or in part.
6. Incorporation of Provisions: The Contractor will include the provisions of paragraphs one through six in every subcontract, including procurements of materials and leases of equipment, unless exempt by the Acts, the Regulations, and directives issued pursuant there to. The Contractor will take action with respect to any subcontract or procurement as the Sponsor or the Federal Aviation Administration may direct as a means of enforcing such provisions including sanctions for noncompliance. Provided, that if the Contractor becomes involved in, or is threatened with

litigation by a subcontractor, or supplier because of such direction, the Contractor may request the Sponsor to enter into any litigation to protect the interests of the Sponsor. In addition, the Contractor may request the United States to enter into the litigation to protect the interests of the United States. The above provision binds the Contractor and subcontractors from the bid solicitation period through the completion of the contract.

*In addition -The Airport must include this provision as a covenant running with the land, in any future deeds, leases, licenses, permits, or similar instruments entered into by the Sponsor with other parties for all transfers of real property acquired or improved under Airport Improvement Program*

*This applies to agreements such as leases where a physical portion of the airport is transferred for use, for example a fuel farm, apron space, or a parking facility. MANDATORY TEXT*

#### Clauses for Transfer of Real Property Acquired or Improved Under the Airport Improvement Program

The following clauses will be included in deeds, licenses, leases, permits, or similar instruments entered into by the Sponsor pursuant to the provisions of the Airport Improvement Program grant assurances:

- A. The (grantee, lessee, permittee, etc. as appropriate) for himself/herself, his/her heirs, personal representatives, successors in interest, and assigns, as a part of the consideration hereof, does hereby covenant and agree [in the case of deeds and leases add “as a covenant running with the land”] that:
  1. In the event facilities are constructed, maintained, or otherwise operated on the property described in this (deed, license, lease, permit, etc.) for a purpose for which a Federal Aviation Administration activity, facility, or program is extended or for another purpose involving the provision of similar services or benefits, the (grantee, Guidelines for Contract Provisions for Obligated Sponsors and Airport Improvement Program Projects Issued on January 20, 2023 Page 25 licensee, lessee, permittee, etc.) will maintain and operate such facilities and services in compliance with all requirements imposed by the Nondiscrimination Acts and Regulations listed in the Title VI List of Pertinent Nondiscrimination Acts and Authorities (as may be amended) such that no person on the grounds of race, color, or national origin, will be excluded from participation in, denied the benefits of, or be otherwise subjected to discrimination in the use of said facilities.
- B. With respect to licenses, leases, permits, etc., in the event of breach of any of the above Nondiscrimination covenants, (Title of Sponsor) will have the right to terminate the (lease, license, permit, etc.) and to enter, re-enter, and repossess said lands and facilities thereon, and hold the same as if the (lease, license, permit, etc.) had never been made or issued.\*
- C. With respect to a deed, in the event of breach of any of the above Nondiscrimination covenants, the (Title of Sponsor) will have the right to enter or re-enter the lands and facilities thereon, and the above-described lands and facilities will there upon revert to and vest in and become the absolute property of the (Title of Sponsor) and its assigns.\*

*(\*Reverter clause and related language to be used only when it is determined that such a clause is necessary to make clear the purpose of Title VI.)*

*The Airport must include this provision in any future (deeds, leases, licenses, permits, or similar instruments) entered into by the Sponsor/Airport with other parties for the construction or use of, or access to, space on, over, or under real property acquired or improved under Airport Improvement Program*

*This applies to agreements such as leases of concession space in a terminal MANDATORY TEXT*

#### Clause for Construction/Use/Access to Real Property Acquired Under the Activity, Facility or Program

The following clauses will be included in deeds, licenses, permits, or similar instruments/agreements entered into by **(Title of Sponsor)** pursuant to the provisions of the Airport Improvement Program grant assurances.

- A. The (grantee, licensee, permittee, etc., as appropriate) for himself/herself, his/her heirs, personal representatives, successors in interest, and assigns, as a part of the consideration hereof, does hereby covenant and agree (in the case of deeds and leases add, "as a covenant running with the land") that (1) no person on the ground of race, color, or national origin, will be excluded from participation in, denied the benefits of, or be otherwise subjected to discrimination in the use of said facilities, (2) that in the construction of any improvements on, over, or under such land, and the furnishing of services thereon, no person on the ground of race, color, or national origin, will be excluded from participation in, denied the benefits of, or otherwise be subjected to discrimination, (3) that the (grantee, licensee, lessee, permittee, etc.) will use the premises in compliance with all other requirements imposed by or pursuant to the Title VI List of Pertinent Nondiscrimination Acts and Authorities.
- B. With respect to (licenses, leases, permits, etc.), in the event of breach of any of the above Non-discrimination covenants, **(Title of Sponsor)** will have the right to terminate the (license, permit, etc., as appropriate) and to enter or re-enter and repossess said land and the facilities Guidelines for Contract Provisions for Obligated Sponsors and Airport Improvement Program Projects Issued on January 20, 2023 Page 26 thereon, and hold the same as if said (license, permit, etc., as appropriate) had never been made or issued.\*
- C. With respect to deeds, in the event of breach of any of the above Non-discrimination covenants, **(Title of Sponsor)** will there upon revert to and vest in and become the absolute property of **(Title of Sponsor)** and its assigns.\*

(\*Reverter clause and related language to be used only when it is determined that such a clause is necessary to make clear the purpose of Title VI.)

LOGAN – CACHE AIRPORT AUTHORITY BOARD  
FEBRUARY 6, 2025

# ATTACHMENT B

CACHE COUNTY GOVERNMENT  
REVENUES WITH COMPARISON TO BUDGET  
FOR THE 1 MONTHS ENDING JANUARY 01, 2025

AIRPORT FUND

		ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEARNED	PCNT
<u>INTERGOVERNMENTAL REVENUE</u>							
277-33-70105	LOGAN CITY-SHARED NET EXP	.00	.00	.00	100,000.00	100,000.00	.0
	TOTAL INTERGOVERNMENTAL REVE	.00	.00	.00	100,000.00	100,000.00	.0
<u>MISCELLANEOUS REVENUE</u>							
277-36-10000	INTEREST	.00	.00	.00	30,000.00	30,000.00	.0
277-36-15000	GAS TAX REFUND	.00	.00	.00	8,000.00	8,000.00	.0
277-36-16000	LANDING FEES	.00	.00	.00	5,500.00	5,500.00	.0
277-36-18000	FUEL FLOW -STORAGE FEES	.00	.00	.00	20,000.00	20,000.00	.0
277-36-19000	FIRE DEPARTMENT STANDBY FEES	.00	.00	.00	5,000.00	5,000.00	.0
277-36-90000	SUNDRY REVENUE	.00	.00	.00	4,000.00	4,000.00	.0
	TOTAL MISCELLANEOUS REVENUE	.00	.00	.00	72,500.00	72,500.00	.0
<u>AIRPORT LAND LEASE REVENUES</u>							
277-37-80000	AIRPORT FEES-LAND LEASE INCOM	.00	.00	.00	113,400.00	113,400.00	.0
	TOTAL AIRPORT LAND LEASE REVE	.00	.00	.00	113,400.00	113,400.00	.0
<u>CONTRIBUTIONS &amp; TRANSFERS</u>							
277-38-20000	CONTRIBUTION - CACHE COUNTY	.00	.00	.00	100,000.00	100,000.00	.0
277-38-90000	APPROPRIATED FUND BALANCE	.00	.00	.00	629,022.00	629,022.00	.0
	TOTAL CONTRIBUTIONS & TRANSFE	.00	.00	.00	729,022.00	729,022.00	.0
	TOTAL FUND REVENUE	.00	.00	.00	1,014,922.00	1,014,922.00	.0

CACHE COUNTY GOVERNMENT  
EXPENDITURES WITH COMPARISON TO BUDGET  
FOR THE 1 MONTHS ENDING JANUARY 01, 2025

AIRPORT FUND

	ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEXPENDED	PCNT
<u>AIRPORT</u>						
277-4460-110 FULL TIME EMPLOYEES	.00	.00	.00	162,673.00	162,673.00	.0
277-4460-115 OVERTIME	.00	.00	.00	3,000.00	3,000.00	.0
277-4460-120 PART TIME EMPLOYEES	.00	.00	.00	62,428.00	62,428.00	.0
277-4460-130 EMPLOYEE BENEFITS	.00	.00	.00	114,521.00	114,521.00	.0
277-4460-210 SUBSCRIPTIONS & MEMBERSHIPS	.00	.00	.00	100.00	100.00	.0
277-4460-220 PUBLIC NOTICES	.00	.00	.00	300.00	300.00	.0
277-4460-230 TRAVEL	.00	.00	.00	4,000.00	4,000.00	.0
277-4460-240 OFFICE SUPPLIES	.00	.00	.00	1,500.00	1,500.00	.0
277-4460-250 EQUIPMENT SUPPLIES & MAINT	.00	.00	.00	46,000.00	46,000.00	.0
277-4460-251 NON CAPITALIZED EQUIPMENT	.00	.00	.00	13,000.00	13,000.00	.0
277-4460-260 BUILDING & GROUNDS	.00	.00	.00	25,000.00	25,000.00	.0
277-4460-261 SNOW REMOVAL	.00	.00	.00	65,000.00	65,000.00	.0
277-4460-262 VEGETATION CONTROL - CHEMICAL	.00	.00	.00	12,000.00	12,000.00	.0
277-4460-263 VEGETATION CONTROL - MOWING	.00	.00	.00	5,000.00	5,000.00	.0
277-4460-270 UTILITIES	.00	.00	.00	27,000.00	27,000.00	.0
277-4460-280 COMMUNICATIONS	.00	.00	.00	5,000.00	5,000.00	.0
277-4460-290 FUEL	.00	.00	.00	10,000.00	10,000.00	.0
277-4460-291 UNION PACIFIC PROPERTY LEASE	.00	.00	.00	20,000.00	20,000.00	.0
277-4460-311 PROFESSIONAL SERVICES	.00	.00	.00	1,000.00	1,000.00	.0
277-4460-330 EDUCATION & TRAINING	.00	.00	.00	44,000.00	44,000.00	.0
277-4460-510 INSURANCE	.00	.00	.00	18,000.00	18,000.00	.0
277-4460-621 MISC BOARD SERVICES/TRAVEL	.00	.00	.00	8,000.00	8,000.00	.0
277-4460-625 LOGAN FIRE - STANDBY FEES	.00	.00	.00	8,000.00	8,000.00	.0
 TOTAL AIRPORT	 .00	 .00	 .00	 655,522.00	 655,522.00	 .0

CACHE COUNTY GOVERNMENT  
EXPENDITURES WITH COMPARISON TO BUDGET  
FOR THE 1 MONTHS ENDING JANUARY 01, 2025

AIRPORT FUND

	ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEXPENDED	PCNT
<u>CONTRIBUTIONS</u>						
277-4800-477 TRANSFER OUT - AIRPORT CAPITAL	.00	.00	.00	359,400.00	359,400.00	.0
TOTAL CONTRIBUTIONS	.00	.00	.00	359,400.00	359,400.00	.0
TOTAL FUND EXPENDITURES	.00	.00	.00	1,014,922.00	1,014,922.00	.0
NET REVENUE OVER EXPENDITURES	.00	.00	.00	.00	.00	.0

CACHE COUNTY GOVERNMENT  
REVENUES WITH COMPARISON TO BUDGET  
FOR THE 1 MONTHS ENDING JANUARY 01, 2025

CAPITAL PROJECT AIRPORT

	ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEARNED	PCNT
477-33-15000 FED GRANT - SCASDP	.00	.00	.00	1,203,000.00	1,203,000.00	.0
477-33-44402 STATE GRANT	.00	.00	.00	862,200.00	862,200.00	.0
TOTAL SOURCE 33	.00	.00	.00	2,065,200.00	2,065,200.00	.0
SOURCE 38						
477-38-10277 TRANSFER IN - AIRPORT	.00	.00	.00	359,400.00	359,400.00	.0
TOTAL SOURCE 38	.00	.00	.00	359,400.00	359,400.00	.0
TOTAL FUND REVENUE	.00	.00	.00	2,424,600.00	2,424,600.00	.0

CACHE COUNTY GOVERNMENT  
EXPENDITURES WITH COMPARISON TO BUDGET  
FOR THE 1 MONTHS ENDING JANUARY 01, 2025

CAPITAL PROJECT AIRPORT

	ENCUMBRANCE	YTD ACTUAL	TOTAL	BUDGET	UNEXPENDED	PCNT
<u>AIRPORT</u>						
477-4460-730 IMPROVEMENTS	.00	.00	.00	2,222,600.00	2,222,600.00	.0
477-4460-740 CAPITALIZED EQUIPMENT	.00	.00	.00	202,000.00	202,000.00	.0
TOTAL AIRPORT	.00	.00	.00	2,424,600.00	2,424,600.00	.0
TOTAL FUND EXPENDITURES	.00	.00	.00	2,424,600.00	2,424,600.00	.0
NET REVENUE OVER EXPENDITURES	.00	.00	.00	.00	.00	.0

LOGAN – CACHE AIRPORT AUTHORITY BOARD  
FEBRUARY 6, 2025

# ATTACHMENT C

2022 lease 2022 lease changes

2024 lease 2024 changes

## LOGAN-CACHE AIRPORT AUTHORITY GROUND LEASE AGREEMENT

This lease is made and entered into by and between the Logan-Cache Airport Authority, which shall be called the “LESSOR” in this agreement and the “LESSEE” as indicated below:

Name: \_\_\_\_\_

Address: \_\_\_\_\_

Phone: \_\_\_\_\_

Email: \_\_\_\_\_

Contact: \_\_\_\_\_

In consideration of the mutual terms and conditions contained in this agreement, the parties hereto do hereby agree as follows:

### A. LEASED PREMISES

Lessor hereby leases to Lessee and Lessee hereby leases from Lessor the following Hangar Site located at the Logan-Cache Airport:

Designated Number: \_\_\_\_\_

Tax ID Number: \_\_\_\_\_

Total Site Hangar Dimensions: \_\_\_\_\_

### B. RENT

1. Lessee shall pay rent to the Lessor for the Hangar Site in the amount of \$\_\_\_\_\_ per year.
2. Annual rent payments shall be payable in advance and due on or before July 1<sup>st</sup> of each year during the term of this lease. For the initial and final lease periods, the amount of rent due will be prorated for the respective periods based on the rental rate due for that period. Lessee agrees to pay a late charge of ten percent (10%) of the amount due for any amount not received within thirty (30) days of the due date.
3. The Lessor and the Lessee agree that the rent due under this agreement shall be increased by two percent (2% per year) at least the social security cost of living percentage per year. All rents due under this agreement will be increased for the cost of living adjustment as of July 1<sup>st</sup> of the year regardless of the specific date when the lease was entered into. Prepaid rent shall not be adjustable and shall be considered rent paid in full for the period prepaid.

4. The Lessee will also be assessed a one-time hookup fee of \$1,000.00 or the current Logan City connection fee, whichever is greater, for sewer and water service.
5. Lessee shall be responsible for all utility services, charges and costs of installation and maintenance. Utility services include but are not limited to water, sewer, power, gas, and telecommunications.

#### C. TERM

1. The initial term of this lease shall be for the following period of 10 years from:  
\_\_\_\_\_ to \_\_\_\_\_  
~~The lease may be renewed under the same terms and conditions for an additional ten year term by Lessee giving Lessor advance written notice at least 180 days prior to expiration of the initial term of this lease.~~
2. The lease may be terminated by either party upon written notice given at least 180 days ~~six (6) months~~ prior to termination.

#### D. IMPROVEMENTS

1. Lessee has the right to construct and maintain the hangar and aviation-related improvements on the premises subject to the terms of this lease.
2. Any hangars or improvements, including any modifications, must comply with the provisions of the Uniform Building Code, Uniform Fire Code, and other uniform codes and standards adopted by the City, as well as any applicable federal or state laws relating to airport structures. No hangar or permitted improvement may be erected or modified without a city building permit having first been obtained by the Lessee and permission obtained from the Lessor. Permission shall not be unreasonably withheld.
3. Upon the termination of this lease, Lessee shall have the right to remove the hangar and any improvements erected by the Lessee; provided, however, that the Lessee, upon such removal, shall leave the Hangar Site clean and free of debris, concrete, litter, abandoned equipment and materials. The removal must be completed within sixty (60) days from the date of termination. Lessor shall have the option, upon receipt of notice from the Lessee of the intention of the Lessee to remove the hangar or improvements, to purchase the hangar or improvements at a fair market value. Lessor shall exercise the option by written notice thereof within thirty (30) days of the notice of intention to terminate.

#### E. RESTRICTIONS

1. The premises' use must be primarily devoted to housing and maintaining aircraft and aviation-related equipment. Peripheral use for storage of other non-hazardous items is allowed. Lessee may not use the premises primarily for non-aviation related purposes.
2. Storage of fuel on premises is not allowed except in regular, built-in aircraft fuel tanks. Fuel dispensing from permanently-installed containers at the fuel farm may be allowed, but only where the tanks and pumps are installed in accordance with fire and building codes, and where the fuel is used only by the Lessee. Selling fuel to other parties is not allowed unless the Lessee meets standards established by the Logan-Cache Airport Authority and pays a dispensing fee to the Logan-Cache Airport Authority.

3. Users may self-fuel their own aircraft in the designated area away from the hangars. Fuel hauled to the airport for this purpose may not be kept inside the hangars. Aircraft are not allowed to be fueled inside the hangars.
4. No signs may be displayed on the exterior of any hangar or improvement, other than the hangar number, without the prior written consent of the Lessor. Such approval shall not be unreasonably withheld.

#### **F. COMPLIANCE WITH APPLICABLE LAWS**

Lessee shall at all times comply with all applicable federal, state, county and city laws, rules, ordinances, and regulations for the use of the hangar, airport facilities, and the airport including, but not limited to, those rules and regulations promulgated by the Federal Aviation Administration, as well as the airport zoning regulations contained in the Master Plan adopted by the city for the Logan-Cache Airport. Any violation of any applicable federal, state, county and city laws, rules, ordinances and regulations shall be deemed a violation of this lease.

#### **G. LESSOR RESERVATIONS**

1. Lessor reserves the exclusive right to develop or improve the airport or any portions thereof and take any necessary action or steps to protect the aerial approaches of the airport against obstructions including, but not limited to, height, building and use restrictions as to the premises leased hereunder if Lessor reasonably deems that the buildings and improvements or the use thereof by the Lessee constitutes an obstruction or danger to the safe operation of the airport.
2. Lessor shall reserve the right to enter any hangar at reasonable times for the purpose of inspecting the premises for Fire Code issues, safety factors and compliance with the Uniform Building Code and other applicable federal, state and county codes and requirements, and to verify the identification and location of aircraft located within the hangar upon ten (10) days notice.

#### **H. DEFAULT**

In the event the Lessee fails to pay any rental payments as required by the terms of this lease or in the event that the Lessee fails to comply with any other provision of this agreement, Lessor shall have the right, after thirty (30) days notice to the Lessee of such default or failure to comply and upon the failure of the Lessee to cure the default, to terminate this agreement and to remove the Lessee from the premises. Upon such removal, the Lessor may retain possession of the Hangar Site premises and lease the same to other parties as it may, in its discretion, deem reasonable and necessary. Upon such termination, the Lessee agrees to peaceably vacate the premises and to remove the hangar, improvements, and any equipment located therein within sixty (60) days from the date of said termination. Upon the failure to remove the hangar, improvements, or equipment within that time period, such hangar, improvements, or equipment shall revert to the Lessor or be removed by the Lessor and Lessee shall be responsible for any and all expenses incurred by Lessor for the removal. Any amounts that are the responsibility of the Lessee are due and payable to Lessor upon presenting proof of the expenses incurred to the Lessee. Lessor retains its option to acquire the hangar and any improvements as provided in Paragraph D.

## **I. PROHIBITION AGAINST ASSIGNMENT**

This lease may not be assigned nor sublet without the prior written consent of the Lessor. Said consent shall not be unreasonably withheld. In the event the hangar is sold, the new owner will be required to execute a new lease agreement **with Lessor.**

## **J. INDEMNIFICATION AND LIABILITY INSURANCE**

1. Lessee shall indemnify and hold the Lessor harmless from any and all damages incurred by Lessee, any of its affiliates, guests, and/or invitees, and Lessee shall indemnify and hold Lessor harmless for any and all damages incurred to the property of the Lessee. Lessee further agrees to indemnify ~~any person or property of the Lessee~~ and to protect and save harmless the Lessor from any liability or expenses of defense or otherwise by reason of any injury to any person or any property upon the premises or surrounding areas during the term of this lease including reasonable attorney's fees and cost.  
~~If Lessee maintains a pre-existing fuel tank upon the premises, Lessee shall obtain and maintain a general liability insurance policy designating the Lessor as a co-insured party with minimum coverage of \$1,000,000.00 general liability. Lessee shall provide Lessor a certificate of insurance on an annual basis showing the above coverage.~~
2. Lessee shall obtain and maintain a general liability insurance policy, in full force and effect at all times during the term of the lease, with minimum general liability coverage of \$1,000,000.00 combined single limit per occurrence. The policies are to contain, or be endorsed to contain the following provisions:
  - a. The Logan-Cache Airport Authority, its officers, officials, employees and volunteers are to be covered as additional insured. The coverage shall contain no special limitations on the scope of protection afforded to the Logan-Cache Airport Authority, its officers, officials, employees and volunteers.
  - b. Lessee's insurance shall be primary insurance as respect to the Logan-Cache Airport Authority, its officers, officials, employees and volunteers. Any insurance maintained by the Logan-Cache Airport Authority, its officers, officials, employees and volunteers shall be in excess of the lessee's insurance and shall not contribute with it.
  - c. Lessee shall provide Lessor a certificate of insurance on an annual basis showing the above coverage. **If no current certificate of insurance is provided to Lessor, the Lessee will be deemed to be in default under this agreement.**

## **K. SUBORDINATION**

This lease shall be subordinate to the provisions of any existing or future agreement between the Lessor and the United States relative to the operation or maintenance of the airport if such agreement is required as a condition precedent to the obtaining or expenditure of federal funds for the development and use of the airport.

## **L. CONSTRUCTION PERFORMANCE**

If Lessee is building a hangar, Lessee agrees to obtain a building permit within ninety (90) (30) days from the date Lessee signs this agreement. It is also agreed that building will commence within one hundred twenty (120) (90) days from the Lessee signing this agreement. An extension on the building of the hangar may be requested from the Lessor. The extension request must be made to the Lessor in writing if more than one hundred twenty (120) days is required. be made to the Lessor in writing if more than one hundred twenty (120) days is required. The hangar must be completed within one hundred eighty (180) days of commencement of construction. The Lessee is responsible for securing the construction site to assure that it is safe for tenants and visitors, and does not obstruct or interfere with business activities at the airport.

#### **M. GOVERNING LAW**

This agreement is to be interpreted in accordance with the laws of the State of Utah.

#### **N. NOTICE**

Should Notice be required under this agreement, any and all correspondence shall be provided in writing to the parties and given by either personal delivery with a signed acknowledgment of receipt; by registered or certified mail, postage prepaid, with return receipt requested; by an established, nationally-recognized commercial courier service, charge prepaid, with written proof of delivery; or by electronic mail with confirmation copy sent by an established, nationally-recognized commercial courier service, as provided above, within 24 hours after the time and date of the electronic mail transmission. Written Notice shall be addressed to the following designated representatives:

**Lessee:**

Name: \_\_\_\_\_

Address: \_\_\_\_\_

Email: \_\_\_\_\_

**Lessor:**

Name: \_\_\_\_\_

Address: \_\_\_\_\_

Email: \_\_\_\_\_

IN WITNESS THEREOF, the parties have executed the agreement in duplicate, each of which shall be deemed an original, on the \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

LESSOR:

Logan-Cache Airport Authority

LESSEE:

\_\_\_\_\_

By: \_\_\_\_\_  
Board Chair

By: \_\_\_\_\_  
Signature

LOGAN – CACHE AIRPORT AUTHORITY BOARD  
FEBRUARY 6, 2025

# ATTACHMENT D

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## Self Serve Storage Tank Reconsideration

1 message

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**Scott Weaver** <sweaver@leaviation.com>

Tue, Feb 4, 2025 at 9:11 AM

To: "bob.low@cachecounty.gov" <bob.low@cachecounty.gov>

Cc: "ryansnowcpa@gmail.com" <ryansnowcpa@gmail.com>, Brett Hugie <Brett.Hugie@cve.com>, "John Kerr - Logan-Cache Airport - UT (kerrjohna@comcast.net)" <kerrjohna@comcast.net>, Janeen Allen <janeen.allen@cachecounty.gov>, "david.zook@cachecounty.gov" <david.zook@cachecounty.gov>, "holly.daines@loganutah.org" <holly.daines@loganutah.org>

Good morning, Mr. Low,

I am requesting the Airport Authority's reconsideration in allowing Leading Edge Aviation to continue with installing a self-serve avgas storage tank on the south end of the ramp. March 2022 the Airport Authority approved Leading Edge Aviation to install a 2,000-gallon avgas self-service fuel tank. Shortly after I began the project Mr. John Kerr and Mr. Bill Francis had asked me to stop the project as they wanted to explore other options, and I complied. Attached is the location site Armstrong approved and my engineering drawing for the tank.

We would like to increase the size of the tank and install a 15,000-gallon avgas self-serve storage tank this Summer.

Thank you for your reconsideration,

Scott Weaver  
Leading Edge Aviation

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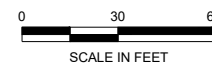
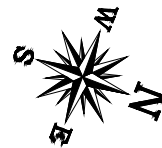
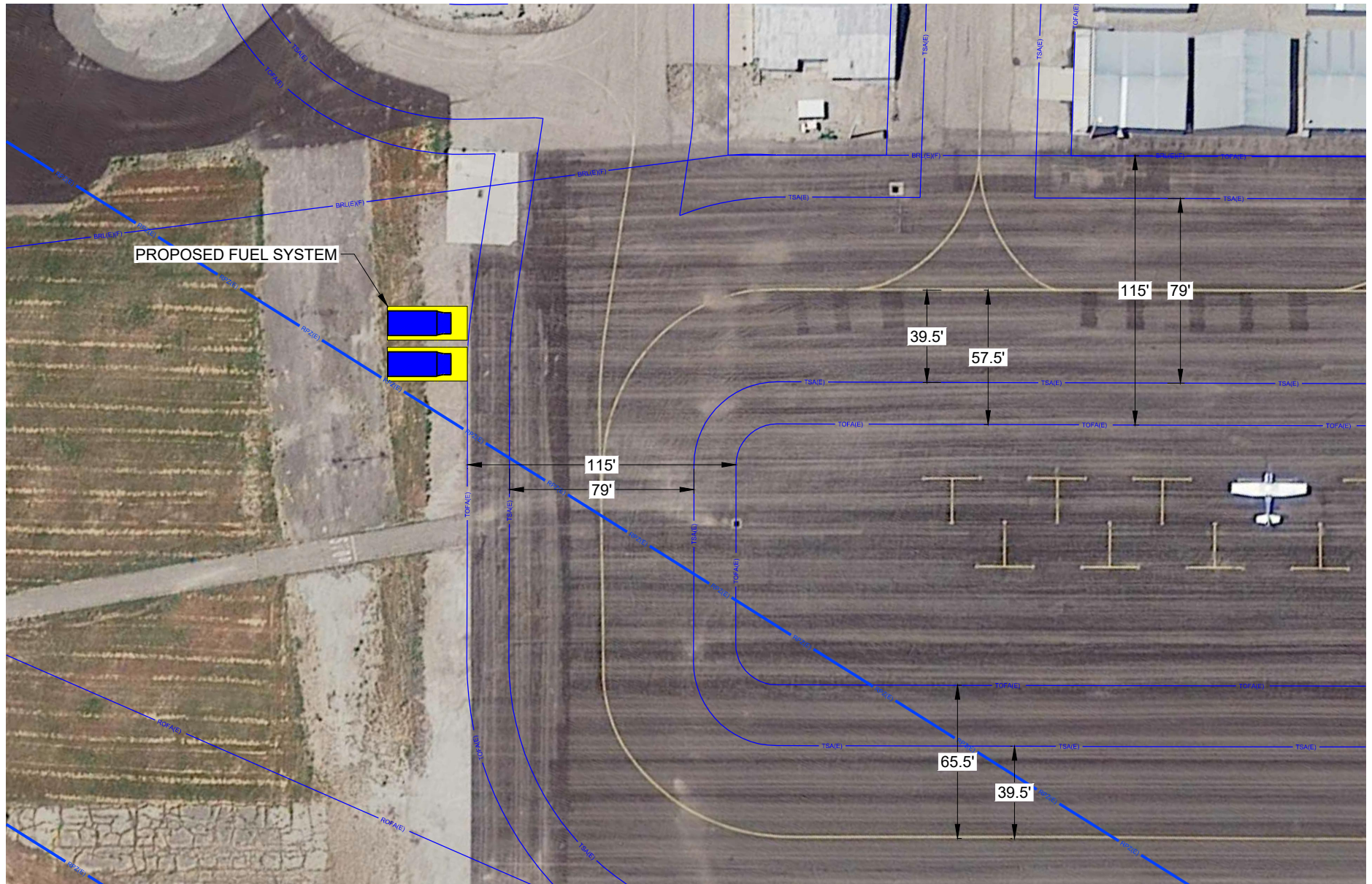
### 2 attachments



**Fuel System-Exhibit.pdf**  
917K



**Logan\_UT Plan Set 011720.pdf**  
9070K



LOGAN-CACHE AIRPORT LOGAN, UTAH
FUEL SYSTEM EXHIBIT
 <b>ARMSTRONG</b> PLANNING ENGINEERING CONSTRUCTION <a href="http://www.armstrongaerobuild.com">www.armstrongaerobuild.com</a>

SHEET INDEX	
SHEET#	SHEET TITLE
T-100	COVER SHEET + SITE DATA
T-110	GENERAL NOTES
T-120	ABBREVIATIONS, LEGEND AND NOTES
C-100	EXISTING CONDITIONS, EROSION & SEDIMENT CONTROL PLAN
C-200	PROPOSED SITE PLAN
D-100	CONSTRUCTION DETAILS
D-200	SIGNAGE & TANK LABELING DETAILS & SPECIFICATIONS
M-100	TANK SHOP DRAWINGS
M-200	FUELING SYSTEM EQUIPMENT SPECIFICATIONS
M-210	FUELING SYSTEM EQUIPMENT SPECIFICATIONS
M-220	FUELING SYSTEM EQUIPMENT SPECIFICATIONS
M-230	FUELING SYSTEM EQUIPMENT SPECIFICATIONS
M-240	FUELING SYSTEM EQUIPMENT SPECIFICATIONS
E-100	ELECTRICAL NOTES
E-200	ELECTRICAL SITE, GROUNDING & BONDING PLAN
E-300	ELECTRICAL DIAGRAMS & PANELBOARD SCHEDULE
E-400	NEC HAZARD ZONE DEFINITION PLAN & DETAILS

SCOPE OF WORK	
1.	INSTALL SEDIMENT & EROSION CONTROL MEASURES AS DEFINED HEREIN IN ACCORDANCE W/ STATE, COUNTY & AIRPORT REQUIREMENTS.
2.	EXCAVATE & PREPARE SOIL FOR NEW SLAB AS DIRECTED HEREIN.
3.	INSTALL NEW ELECTRICAL WIRING AND AS DEFINED HEREIN.
4.	INSTALL NEW REINFORCED TANK SLAB.
5.	INSTALL NEW ELECTRICAL SERVICE CONNECTION.
6.	INSTALL NEW 2,000 GALLON AST W/ DISPENSER, HOSE REEL & ASSOCIATED EQUIPMENT FOR STORAGE & DISPENSING OF AVIATION GASOLINE (AVGAS).

SPECIAL INSPECTIONS	
SPECIAL INSPECTIONS SHALL BE PERFORMED IN ACCORDANCE WITH IBC 2018 CONCRETE CONSTRUCTION (IBC 1704.4)	
1.	INSPECTIONS OF REINFORCING STEEL
2.	PLACEMENT & CURING
3.	FORM WORK
4.	INSPECT BOLTS TO BE INSTALLED IN CONCRETE PRIOR TO & DURING PLACEMENT OF CONCRETE
5.	FRESH CONCRETE SHALL BE SAMPLED TO FABRICATE SPECIMENS FOR STRENGTH TESTS, PERFORM SLUMP & AIR CONTENT TESTS, & DETERMINE TEMPERATURE OF CONCRETE.
6.	INSPECT FORM WORK FOR SHAPE, LOCATION & DIMENSIONS OF CONCRETE MEMBER BEING FORMED.
SOILS (IBC 1704.7)	
1.	SOIL SITE PREPARATION
2.	FILL PLACEMENT
3.	IN PLACE DENSITY
4.	SUBGRADE FOR FOUNDATIONS OR SLABS

DESIGN TEAM / APPLICANT INFORMATION	
	N.D. ERYOU, PhD, PE
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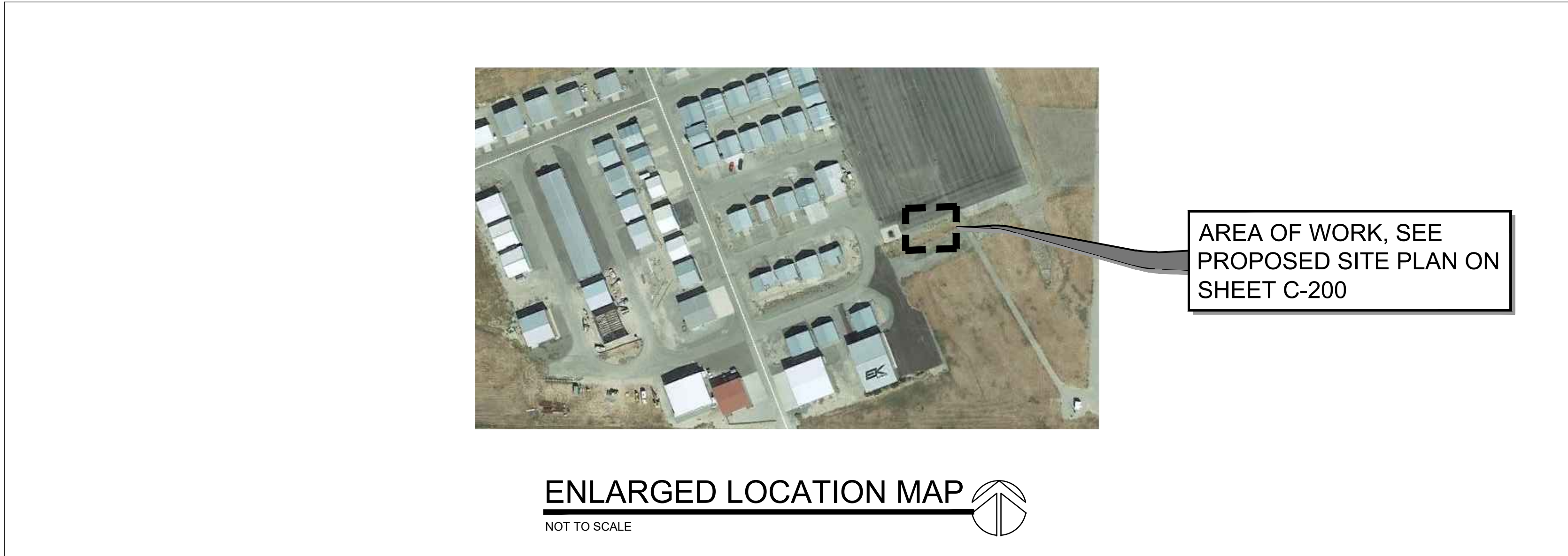
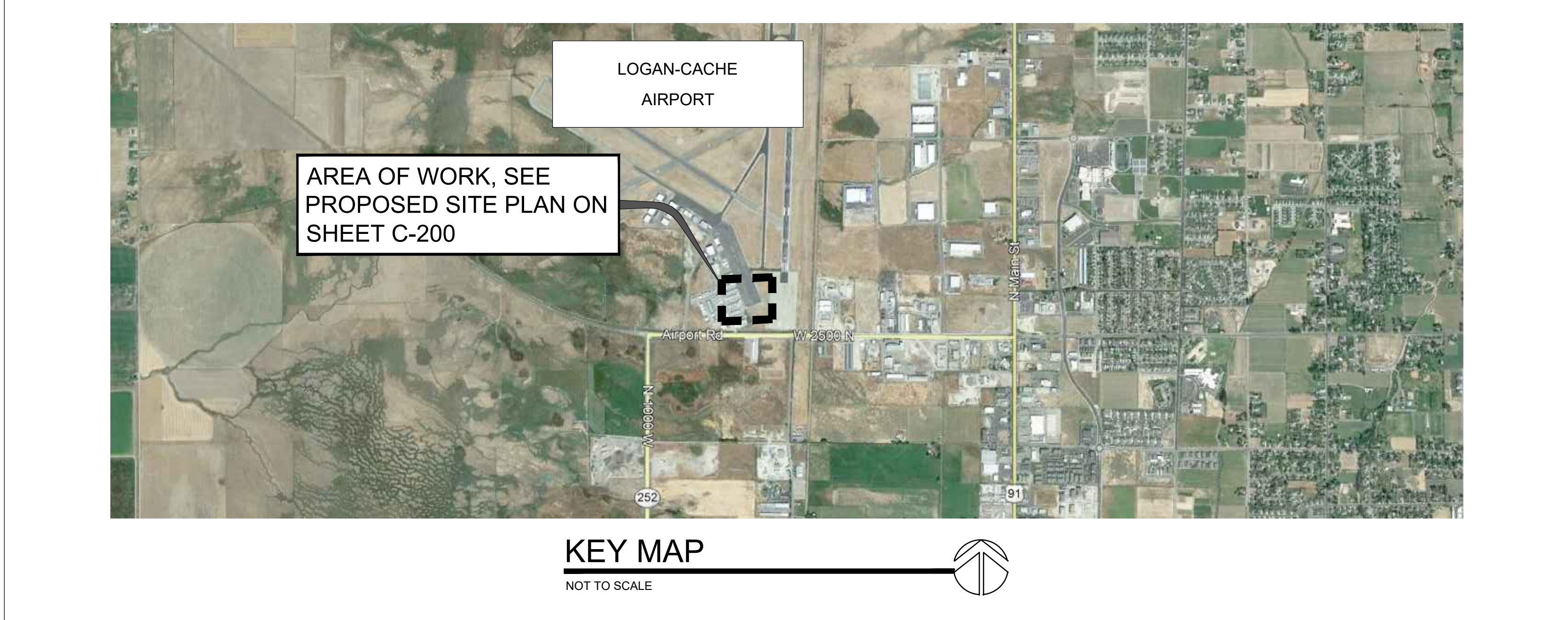
PROPERTY INFORMATION	
LOGAN-CACHE AIRPORT	
2500 NORTH AIRPORT DRIVE	
LOGAN, UTAH 84321	

CONTRACTOR INFORMATION	
AMERICAN ENVIRONMENTAL ASSESSMENT CORPORATION	
3977 AVIATION LOOP	
SANFORD, FLORIDA 32773	
PHONE: (631) 586-2000	
POINT OF CONTACT: PAUL SUNDBY	
PSUNDBY@AMERICAN-ENVIRONMENTAL.NET	

FACILITY OPERATOR INFORMATION	
LEADING EDGE AVIATION	
2500 NORTH AIRPORT DRIVE, SUITE 2	
LOGAN, UTAH 84321	
PHONE: (435) 752-5955	

GENERAL NOTES	
1.	THE CONTRACTOR SHALL REVIEW ALL DOCUMENTS AND VERIFY ALL DIMENSIONS AND FIELD CONDITIONS AND SHALL CONFIRM THAT WORK IS BUILDABLE AS SHOWN. ANY CONFLICTS OR OMISSION, ETC., SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER FOR CLARIFICATION PRIOR TO THE PERFORMANCE OF ANY WORK IN QUESTION.
2.	DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS SHALL GOVERN IN ALL INSTANCES. IN CASE OF CONFLICT, NOTIFY THE ENGINEER.
3.	CONTRACTOR SHALL COORDINATE AND OBTAIN ALL NECESSARY APPROVALS AND INSPECTIONS AS REQUIRED BY LOCAL GOVERNING JURISDICTIONS.
4.	THE CONTRACTOR IS EXPECTED TO KEEP PREMISES CLEAN DURING CONSTRUCTION. TRASH WILL NOT BE ALLOWED TO ACCUMULATE IN THE SPACE DURING CONSTRUCTION. ALL DEBRIS SHALL BE REMOVED DAILY. FINAL CLEAN UP AND REPAIR IS PART OF THIS WORK. REMOVE ALL DUST, DEBRIS, OILS, STAINS, FINGERPRINTS AND LABELS FROM ALL EXPOSED FINISHED SURFACES. SAFE WORKING AREA AND CONDITIONS SHALL BE MAINTAINED AT ALL TIMES.
5.	IT IS THE CONTRACTOR'S RESPONSIBILITY TO FURNISH AND INSTALL ALL WORK FIXTURES AND EQUIPMENT NECESSARY TO COMPLETE PROJECT AS PER PLANS. THE CONTRACTOR IS TO DELIVER PROJECT CLEAN AND READY FOR USE AS APPROVED BY OWNER.
6.	THE CONTRACTOR SHALL VERIFY LOCATIONS OF EXISTING UTILITIES BEFORE COMMENCING WORK. CARE SHALL BE TAKEN TO PROTECT ALL UTILITIES WHICH ARE TO REMAIN.
7.	ALL REQUESTS FOR SUBSTITUTIONS OF ANY SPECIFIED ITEMS SHALL BE SUBMITTED IN WRITING TO THE ENGINEER AND WILL BE CONSIDERED ONLY IF THE ALTERNATE PROPOSED IS PROVEN TO BE MORE ADVANTAGEOUS TO THE OWNER WITH RESPECT TO DELIVERY DATE, QUALITY, OR COST. UNDER NO CIRCUMSTANCES WILL THE ENGINEER BE REQUIRED TO PROVE THAT A PRODUCT PROPOSED FOR SUBSTITUTION IS OR IS NOT OF EQUAL QUALITY TO THE PRODUCT SPECIFIED.
8.	PERFORM ALL WORK IN ACCORDANCE WITH ACCEPTED CONSTRUCTION STANDARDS. ALL CONSTRUCTION SHALL BE IN STRICT ACCORDANCE WITH THE PLANS AND SPECIFICATIONS UNLESS A VARIANCE IS APPROVED BY THE LANDLORD AND/OR ENGINEER.
9.	CONTRACTOR SHALL BE RESPONSIBLE FOR A COMPLETE AND OPERATIONAL TENANT SPACE, INCLUDING ALL FINISHES, MECHANICAL, ELECTRICAL AND PLUMBING SYSTEMS AND FIRE PROTECTION SYSTEMS.
10.	ALL WORK PERFORMED SHALL BE IN STRICT COMPLIANCE WITH GOVERNING FEDERAL, STATE, AND LOCAL BUILDING CODE REQUIREMENTS, EXECUTED IN ACCORDANCE WITH ACCEPTED INDUSTRY STANDARDS AND SHALL CONFORM TO SPECIFIC REGULATIONS AS MANDATED BY THE OWNER, THE TENANT, AND THE ENGINEER.
11.	THESE DRAWINGS ARE THE PROPERTY OF N.D. ERYOU, PHD, PE CONSULTING ENGINEER, INC. AND ARE NOT TO BE REPRODUCED OR COPIED IN WHOLE OR PART. THESE DRAWINGS ARE TO BE USED FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN AND ARE NOT TO BE USED ON ANY OTHER PROJECT.

# INSTALLATION OF A NEW ABOVE GROUND FUEL TANK FOR LEADING EDGE AVIATION @ LOGAN-CACHE AIRPORT 2500 N. 900W., LOGAN, UT 84321



CODE REFERENCES		RECOGNIZED STANDARDS & GUIDELINES	
– INTERNATIONAL BUILDING CODE (2018)		– UTAH DEPARTMENT OF ENVIRONMENTAL QUALITY	
– INTERNATIONAL FUEL GAS CODE (2018)		– INTERNATIONAL BUILDING CODE (2018)	
– ICC INTERNATIONAL MECHANICAL CODE (2018)		– NATIONAL ELECTRICAL CODE, ARTICLE 515, BULK STORAGE PLANTS	
– INTERNATIONAL FIRE CODE (2018)		– NFPA 407, AIRCRAFT FUEL SERVICING	
– NATIONAL ELECTRICAL CODE, ARTICLE 515, BULK STORAGE PLANTS		– NFPA 30, FLAMMABLE AND COMBUSTIBLE LIQUIDS CODE	
– NFPA ~ NATIONAL ELECTRICAL CODE (2018)		– AMERICAN PETROLEUM INSTITUTE, #1542~AIRPORT EQUIPMENT MARKING	
– NFPA~1 ~ LIFE SAFETY CODE		– FAA ADVISORY CIRCULAR 150/5300-13, AIRPORT DESIGN	
– NFPA 704 ~ STANDARD FOR IDENTIFICATION OF HAZARDOUS MATERIALS			
– NFPA 407 ~ AIRCRAFT FUEL SERVICING			
– NFPA 30 ~ FLAMMABLE AND COMBUSTIBLE LIQUIDS CODE			
– NFPA 70 ~ NATIONAL ELECTRIC CODE (2011)			
– AMERICAN PETROLEUM INSTITUTE, #1529 (AVIATION FUELING HOSE)			
– AMERICAN PETROLEUM INSTITUTE, #1542 (AIRPORT EQUIPMENT MARKING)			
– LOCAL UTILITY AUTHORITY REGULATIONS			
– (IECC) INTERNATIONAL ENERGY CONSERVATION CODE (2018)			

ISSUES/REVISIONS		No.	Date
Description			

NOTICE  
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS ACTING UNDER THE DIRECTION OF A PROFESSIONAL ENGINEER, OR LICENSED ARCHITECT, TO ALTER THIS DRAWING

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	Southwest Florida Office	5051 Castello Drive, Suite 244 Naples, Florida 34103 Phone: (352) 684-7275 Fax (800) 660-6724 Email: alex@eryouengineering.com

	
LEADING EDGE AVIATION 2500 NORTH AIRPORT DRIVE LOGAN, UTAH 81321	
	
3977 AVIATION LOOP, SANFORD, FLORIDA 32773 PHONE: (631) 586-2000	
INSTALLATION OF A NEW ABOVE GROND FUEL TANK LEADING EDGE AVIATION LOGAN-CACHE AIRPORT 2500 NORTH AIRPORT DRIVE LOGAN, UTAH 81321	
SHEET DESCRIPTION:	
COVER SHEET & SITE DATA	
SEAL & SIGNATURE	DATE: DECEMBER 2019
	PROJECT NO.: AEAC-LOGAN
	DRAWING BY: MSK
	CHK. BY: AGN
DWG No:	
T-100	
BRIAN E. LEWIS, P.E. UTAH P.E. # 5013586-2203 EXP. DATE: 3/31/2021	
CADD FILE NO. LOGAN-CACHE-Set.dwg	

1. SCOPE OF WORK

**THE INTENT OF THE PROJECT IS TO PROVIDE NEW FUEL STORAGE TANK & DISPENSING EQUIPMENT FOR DIRECT TO AIRCRAFT FUELING (AVGAS).**

- INSTALL NEW AVGAS TANK, DISPENSER & HOSE REEL AS INDICATED HEREIN.**

2. TANK:  
INSTALL THE FOLLOWING TANK:

- NEW 2,000 GALLON AST W/ DISPENSER, HOSE REEL & ASSOCIATED EQUIPMENT AD DEFINED HEREIN.

3. DISPENSERS (AVGAS):  
DISPENSER PROPOSED HEREIN FOR THE DIRECT TO AIRCRAFT DELIVERY OF AVGAS SHALL BE INSTALLED W/ A SHEAR VALVE @ BASE PER CODE REQUIREMENTS.

DISPENSER WILL BE CONNECTED TO NEW 75' HOSE REEL W/DRY BREAK DISCONNECT & 1" OVER WING NOZZLE.

DISPENSER SHALL BE INSTALLED W/ FILTER VESSEL BETWEEN DISPENSER & HOSE REEL.

4. AVIATION FUEL RECOVERY UNITS: :  
1 FUEL RECOVERY UNIT SHALL BE INSTALLED FOR USE WITH AVGAS AST AT FUEL FARM. (SUMP SAVER)

5. OVERFILL ALARMS & MECHANICAL SHUT DOWN:  
TANKS SHALL BE INSTALLED WITH AN OVERFILL ALARM PREVENTION SYSTEM IN PLACE WHICH PROVIDES FOR AN AUDIBLE ALARMS WHEN INDIVIDUAL TANKS REACH 90% CAPACITY. FILL PIPING AT ALL TANKS PROPOSED HEREIN IN SHALL BE AFFIXED WITH A HIGH LEVEL SHUT OFF VALVE WHICH MECHANICALLY STOPS FLOW TO TANK WHEN 95% CAPACTY IS REACHED.

6. ELECTRICAL:  
NEW MAIN POWER PANEL TO BE PROVIDED AS INDICATED.  
FUEL FARM PANEL TO BE LOCATED OUTSIDE CLASS I AREA. ALL SWITCH GEAR & COMPONENTS MUST MEET NEC AND LOCAL CODE REQUIREMENTS.

ASME (AMERICAN SOCIETY OF MECHANICAL ENGINEERS)  
ASME 31.3 PROCESS PIPING CODE

NFPA (NATIONAL FIRE PROTECTION ASSOCIATION)  
NFPA 30 FLAMMABLE AND COMBUSTIBLE LIQUIDS CODE (2008)  
NFPA 70 NEC NATIONAL ELECTRIC CODE  
NFPA 407 STANDARD FOR AIRCRAFT FUEL SERVICING  
NFPA 410 STANDARD ON AIRCRAFT MAINTENANCE

ENVIRONMENTAL PROTECTION AGENCY (EPA)  
FACILITY RESPONSE PLAN FOR SPILL PREVENTION COUNTER-MEASURE AND CONTROL PLAN (SPCC)

40 CFR PART 112 OIL POLLUTION PREVENTION

FAA (FEDERAL AVIATION AUTHORITY)  
FAA AC/150/5230-4B AIRCRAFT FUEL STORAGE, HANDLING AND DISPENSING ON AIRPORTS (DRAFT)  
FAA 7460-1 FORM NOTICE OF PROPOSED CONSTRUCTION

AMERICAN PETROLEUM INSTITUTE (API)  
API-1541-IDENTIFICATION MARKINGS FOR DEDICATED AVIATION FUEL MANUFACTURING AND DISTRIBUTION FACILITIES, AIRPORT STORAGE, AND MOBILE FUELING EQUIPMENT.  
API-1581-SPECIFICATION AND QUALIFICATION PROCEDURES FOR AVIATION JET FUEL FILTER/SEPARATOR (LISTED IN ATA 103)

FUELING SYSTEM IS TO MEET THE FOLLOWING RECOGNIZED STANDARDS AND GUIDELINES:

- UTAH DEPARTMENT OF ENVIRONMENTAL QUALITY
- INTERNATIONAL BUILDING CODE (2018)
- NATIONAL ELECTRICAL CODE, ARTICLE 515, BULK STORAGE PLANTS
- NFPA 407, AIRCRAFT FUEL SERVICING
- NFPA 30, FLAMMABLE AND COMBUSTIBLE LIQUIDS CODE
- AMERICAN PETROLEUM INSTITUTE, #1542~AIRPORT EQUIPMENT MARKING
- FAA ADVISORY CIRCULAR 150/5300-13, AIRPORT DESIGN

2. NEW AST TANKS SHALL BE UL-2085 LISTED,AND MEET THE REQUIREMENTS OF ALL OTHER AUTHORITIES HAVING JURISDICTION.

AVGAS PUMP / FILTER SPECIFICATIONS AND TEST REQUIREMENTS:

1. AVGAS PUMP/FILTER ASSEMBLIES SHALL MEET NATA, ATA, AND API STANDARDS AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH ACCEPTED INDUSTRY PRACTICES. ALL PIPING SHALL BE TESTED AT 75 PSI., AND ALL WIRING SHALL BE IN ACCORDANCE WITH NFPA/NEC REQUIREMENTS.

TESTING:

1. AIR PRESSURE TEST PIPING @ 75 PSI FOR 60 MINUTES.

2. CONFIRM PROPER OPERATION OF TANK INVENTORY CONTROL SYSTEM AND TANK OVERFILL ALARMS AND JET FUEL TANK FILL VALVE PROXIMITY SWITCHES.

3. FUEL HANDLING MODULES TO BE COMMISSIONED BY EQUIPMENT MANUFACTURER, WITNESSED BY THE FUEL SUPPLIER AND THE ENGINEER.

4. ALL TANKS SHOP TESTED WITH 3-5 PSI AIR PRESSURE & DOUBLE WALL TESTING PROCEDURES.

5. ADDITIONALLY, TANK TIGHTNESS TESTING PERFORMED PER VACUUM TEST (2.6 HG) ON INTERSTIS AT MANUFACTURER'S FACILITY WHEN SHIPPING, RE-VERIFIED AT ARRIVAL TO FACILITATE SITE AND HELD THROUGH THE COMPLETION OF INSTALLATION.

PIPING:

1. AVGAS HORIZONTAL AST'S SHALL BE FITTED WITH FUEL RESISTANT EPOXY INTERIOR COATING. TANK MFG SHALL PROVIDE FOR SUMP DRAIN TO REMOVE WATER FROM TANKS.

2. LOADING AND UNLOADING (DISPENSING) CONNECTIONS ARE TO BE PAINTED AS PER API 1542:  
AVGAS - RED BACKGROUND, WHITE LETTERS "AVGAS 100LL". BLUE BANDING ON SILVER.

3. ALL METAL IN CONTACT WITH AVIATION FUEL TO BE FREE OF ZINC, CADMIUM, COPPER, AND THEIR ALLOYS.

4. STEEL PIPING TO BE SCHEDULE 10 TYPE 304 SS, WITH WELDED FLANGED JOINTS.

5. FOR THREADED PIPE COUPLINGS, TEFLON TAPE OR TEFLON PIPE DOPE IS TO BE USED.

6. FOR FLANGED FITTINGS, SYNTHETIC GASKET MATERIAL, IS TO BE COMPATIBLE WITH THE FUEL PRODUCT BEING HANDLED. (BUNA-N, TEFLON, OR VITON-A OR GARLOCK 3000)

7. STATIC ELECTRICITY GROUNDING CABLES MUST BE PROVIDED AT THE TRUCK UNLOADING & LOADING STATIONS.

8. PIPING IS DIAGRAMMATIC ONLY. MANUFACTURER IS RESPONSIBLE FOR ACTUAL LAYOUT AND PROPER SUPPORT OF PIPING SYSTEM.

CODE REFERENCES
<ul style="list-style-type: none"><li>INTERNATIONAL BUILDING CODE (2018)</li><li>INTERNATIONAL FUEL GAS CODE (2018)</li><li>ICC INTERNATIONAL MECHANICAL CODE (2018)</li><li>INTERNATIONAL FIRE CODE (2018 )</li><li>NATIONAL ELECTRICAL CODE, ARTICLE 515, BULK STORAGE PLANTS</li><li>NFPA ~ NATIONAL ELECTRICAL CODE (2018)</li><li>NFPA-1 ~ LIFE SAFETY CODE</li><li>NFPA 704 ~ STANDARD FOR IDENTIFICATION OF HAZARDOUS MATERIALS</li><li>NFPA 407 ~ AIRCRAFT FUEL SERVICING</li><li>NFPA 30 ~ FLAMMABLE AND COMBUSTIBLE LIQUIDS CODE</li><li>NFPA 70 ~ NATIONAL ELECTRIC CODE (2011)</li><li>AMERICAN PETROLEUM INSTITUTE, #1529 (AVIATION FUELING HOSE)</li><li>AMERICAN PETROLEUM INSTITUTE, #1542 (AIRPORT EQUIPMENT MARKING)</li><li>LOCAL UTILITY AUTHORITY REGULATIONS</li><li>(IECC) INTERNATIONAL ENERGY CONSERVATION CODE (2018)</li></ul>

RECOGNIZED STANDARDS & GUIDELINES
<ul style="list-style-type: none"><li>UTAH DEPARTMENT OF ENVIRONMENTAL QUALITY</li><li>INTERNATIONAL BUILDING CODE (2018)</li><li>NATIONAL ELECTRICAL CODE, ARTICLE 515, BULK STORAGE PLANTS</li><li>NFPA 407, AIRCRAFT FUEL SERVICING</li><li>NFPA 30, FLAMMABLE AND COMBUSTIBLE LIQUIDS CODE</li><li>AMERICAN PETROLEUM INSTITUTE, #1542~AIRPORT EQUIPMENT MARKING</li><li>FAA ADVISORY CIRCULAR 150/5300-13, AIRPORT DESIGN</li></ul>

GENERAL:

1. ALL WORK TO BE IN STRICT ACCORDANCE WITH ALL APPLICABLE FEDERAL, STATE, COUNTY, AND LOCAL CODES AND IN STRICT ACCORDANCE WITH APPLICABLE NATIONAL FIRE PROTECTION ASSOCIATION AND NATIONAL ELECTRICAL CODE SPECIFICATIONS AND ASME 31.3 (PROCESS PIPING CODE)

2. ALL MATERIALS SHALL MEET OR EXCEED APPLICABLE MANUFACTURERS WRITTEN SPECIFICATIONS.

3. ALL EQUIPMENT SHALL BE INSTALLED / PIPED / WIRED IN STRICT COMPLIANCE WITH THE REQUIREMENTS AND RECOMMENDATIONS OF THE EQUIPMENT MANUFACTURER.

4. A CLEARLY IDENTIFIED AND EASILY ACCESSIBLE EMERGENCY SWITCH SHALL BE PROVIDED AT A LOCATION AT LEAST 20' BUT NOT MORE THAN 100' FROM THE DISPENSER(S) TO ALLOW FOR POWER SHUTOFF IN THE EVENT OF AN EMERGENCY.

5. NO SMOKING / STOP ENGINE SIGN TO BE POSTED IN DISPENSING AREAS.

6. THE PRODUCT NAME AND HAZARDOUS MATERIAL SIGN SHALL BE STENCILLED ON TANK SO AS TO BE VISIBLE FROM GRADE AFTER INSTALLATION.

7. TANK FILL TO BE COLOR CODED USING AMERICAN PETROLEUM INSTITUTE SYSTEM OF IDENTIFICATION AS IMPLEMENTED BY LOCAL CODE.

8. INSTALLER SHALL FURNISH WRITTEN APPROVAL OF COMPLETED INSTALLATION FROM ALL GOVERNING AGENCIES.

9. ALL PIPING TO BE ADEQUATELY SUPPORTED TO REDUCE THE POSSIBILITY OF DAMAGE DUE TO EXCESS STRESS, DEFLECTION, ETC.

10. TANK / DIKE ASSEMBLY TO BE LISTED AND LABELED BY UNDERWRITERS LABORATORIES,

11. ALL NEW TANKS TO BE FACTORY PRESSURE TESTED FOR LEAKAGE AT 3-5 PSIG PRIOR TO BEING FILLED WITH PRODUCT OR PLACED INTO OPERATION.

12. ALL UNUSED TANK TAPPING ARE TO BE PLUGGED.

13. PRIMARY PIPING TO BE AIR TESTED AT 75 PSIG FOR 60 MIN. ALL JOINTS TO BE CHECKED FOR LEAKAGE USING A SOAP SOLUTION. THE TANK MUST BE VENTED TO ATMOSPHERE PRIOR TO PRESSURIZING PIPING.

14. CONTRACTOR SHALL PROTECT ALL EXISTING STRUCTURES AND EQUIPMENT ADJACENT TO THE WORK AND PROJECT, SUPPORT AND RELOCATE, IF NECESSARY, ALL EXPOSED LINES AND MAKE COMPLETE RESTORATION OF DAMAGED PIPING, CONDUIT, WIRING, CABLES AND APPURTENANCES AT NO COST TO THE OWNER OF SAID UTILITIES, AUTHORITY, OR THE AIRPORT AUTHORITY.

15. THESE DRAWINGS ARE DIAGRAMMATIC IN NATURE AND INDICATE THE GENERAL ARRANGEMENT OF THE VARIOUS SYSTEMS AND THE APPROXIMATE RELATIVE LOCATIONS OF THE EQUIPMENT / DEVICES / ITEMS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THAT THERE IS ADEQUATE SPACE AT THE LOCATIONS INDICATED FOR ALL THE EQUIPMENT / DEVICES / ITEMS PRIOR TO INSTALLATION OF SAME. IF PLAN LAYOUT SPAONG OR INTENT IS CHANGED, THESE CHANGES MUST BE BROUGHT TO THE ATTENTION OF THE ENGINEER FOR APPROVAL.

16. ALL EQUIPMENT AND COMPONENTS SHALL BE PROPERLY GROUNDED AS INDICATED ON THE DRAWINGS AND/OR AS REQUIRED IN ACCORDANCE WITH THE NATIONAL ELECTRICAL CODE.

17. ALL ELECTRICAL CONDUITS ARE SHOWN DIAGRAMMATICALLY. EXACT RUNS SHALL BE DETERMINED BY THE CONTRACTOR IN THE FIELD, EXCEPT WHERE SPECIFICALLY DIMENSIONED ON THE PLAN.

18. ALL EXPOSED CONDUIT SHALL BE PROPERLY SUPPORTED BY APPROVED HANGERS OF ANGLE OR CHANNEL CONSTRUCTION.

19. EXACT CONDUIT STUB UP LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR BASED ON THE MANUFACTURER'S DRAWINGS OF THE RESPECTIVE EQUIPMENT. CONDUITS SHALL BE INSTALLED TO MATCH THE EQUIPMENT FURNISHED.

20. ALL 3 PHASE MOTOR STARTERS SHALL BE NEMA SIZE 1 EXCEPT AS NOTED.

21. FOR AREAS CLASSIFIED AS CLASS 1, DIV 1, ALL ELECTRICAL MATERIALS SHALL BE OF EXPLOSION PROOF CONSTRUCTION. ALL CONDUIT ENTERING THIS AREA SHALL INCLUDE SEAL OFF FITTING. ALL WORK TO BE CONDUCTED IN ACCORDANCE WITH THE LATEST EDITION OF THE NATIONAL ELECTRICAL CODE.

22. ALL OUTDOOR ELECTRICAL ENCLOSURES INCLUDING WIREWAY SHALL BE TYPE NEMA 4X.

23. ALL EXPOSED CARBON STEEL TANK SURFACES SHALL BE COATED AT A MINIMUM WITH A PRIMER COAT, A BOND COAT AND ONE OR MORE FINAL COATS OF PAINT. APPLICATION METHODS SHALL MEET THE REQUIREMENTS OF THE STEEL STRUCTURES PAINTING COUNCIL AND THE NATIONAL ASSOCIATION OF CORROSION ENGINEERS. CONTRACTOR SHALL PROVIDE PAINT / COATING SPECIFICATIONS FOR TANKS, PIPING & SKIDS FOR ENGINEER REVIEW.

24. PRIOR TO ANY TRENCHING EXCAVATION, SOIL BORINGS AND/OR UNDERGROUND EXPLORATION, THE CONTRACTOR SHALL NOTIFY ALL UTILITIES.


25. PRIOR TO CONSTRUCTION, THE CONTRACTOR(S) SHALL BE RESPONSIBLE FOR THE LEGAL TONING & MARKING OUT OF THE SITE TO ENSURE THERE ARE NO UNKNOWN UTILITIES THAT MAY EXIST IN THE WORK AREA.

26. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS & SPECIFICATIONS FOR ALL NEW EQUIPMENT TO ENGINEER OF RECORD FOR REVIEW & APPROVAL PRIOR TO INSTALL.


ISSUES/REVISIONS	No.	Description	Date																	

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LEADING EDGE AVIATION  
2500 NORTH AIRPORT DRIVE  
LOGAN, UTAH 81321



3977 AVIATION LOOP, SANFORD, FLORIDA 32773  
PHONE: (631) 586-2000

INSTALLATION OF A NEW ABOVE GROND FUEL TANK  
LEADING EDGE AVIATION  
LOGAN-CACHE AIRPORT  
2500 NORTH AIRPORT DRIVE  
LOGAN, UTAH 81321

SHEET DESCRIPTION:

GENERAL NOTES

SEAL & SIGNATURE	DATE: DECEMBER 2019
	PROJECT NO.: AEAC-LOGAN
	DRAWING BY: MSK
	CHK. BY: AGN
	DWG No: T-110

BRIAN E. LEWIS, P.E.  
UTAH P.E. # 5013586-2203  
EXP. DATE: 3/31/2021

CADD FILE NO.  
LOGAN-CACHE-Set.dwg



## [edit]

Materials that will not burn under typical fire conditions (e.g. <b>carbon dioxide</b> ), including intrinsically noncombustible materials such as concrete, stone and sand (Materials that will not burn in air when exposed to a temperature of [820 °C (1,500 °F)] for a period of 5 minutes)
Materials that require considerable preheating, under all ambient temperature conditions, before ignition and combustion can occur (e.g. <b>mineral oil</b> ). Includes some finely divided suspended solids that do not require heating before ignition can occur. Flash point at or above 93 °C (200 °F).
Must be moderately heated or exposed to relatively high ambient temperature before ignition can occur (e.g. <b>diesel fuel</b> ) and some finely divided suspended solids that do not require heating before ignition can occur. Flash point between 38 and 93 °C (100 and 200 °F).
Liquids and solids (including finely divided suspended solids) that can be ignited under almost all ambient temperature conditions (e.g. <b>gasoline</b> ). Liquids having a flash point below 23 °C (73 °F) and having a boiling point at or above 38 °C (100 °F) or having a flash point between 23 and 38 °C (73 and 100 °F).
Will rapidly or completely vaporize at normal atmospheric pressure and temperature, or is readily dispersed in air and will burn readily (e.g. <b>acetylene, diethylzinc</b> ). Includes <b>pyrophoric</b> substances. Flash point below 23 °C (73 °F).

Poses no health hazard, no precautions necessary and would offer no hazard beyond that of ordinary combustible materials (e.g. wood)
Exposure would cause irritation with only minor residual injury (e.g. acetone)
Intense or continued but not chronic exposure could cause temporary incapacitation or possible residual injury (e.g. diethyl ether)
Short exposure could cause serious temporary or moderate residual injury (e.g. chlorine)
Very short exposure could cause death or major residual injury (e.g. hydrogen cyanide, phosphine, carbon monoxide, sarin, hydrofluoric acid)

reactive with water (e.g. [helium](#))

and pressures (e.g. [propene](#))

and pressures, reacts violently with water, or may form explosive mixtures with water (e.g. [white phosphorus](#), [potassium](#), [sodium](#))















strong initiating source, must be heated under confinement before initiation, reacts explosively with water, or will detonate if severely shocked (e.g. [ammonium nitrate](#), [chlorine trifluoride](#))

at normal temperatures and pressures (e.g. [nitroglycerin](#), [chlorine azide](#), [chlorine dioxide](#))

	Oxidizer, allows chemicals to burn without an air supply (e.g. potassium perchlorate, ammonium nitrate, hydrogen peroxide).
	Reacts with water in an unusual or dangerous manner (e.g. cesium, sodium, sulfuric acid).
	Simple asphyxiant gas. Specifically limited to the following gases: nitrogen, helium, neon, argon, krypton and xenon. <sup>[2]</sup>

ANGLE	D.S.P.	DRY STANDPIPE	HORIZ.	HORIZONTAL	RESIL.	RESILIENT
AT	DWG.	DRAWINGS	HR.	HOOR	RM.	ROOM
CENTERLINE	E	EXISTING	HGT.	HEIGHT	R.O.	ROUGH OPENING
DIAMETER	E	EACH	I.D.	INSIDE DIAMETER (DIM.)	RWD.	REDWOOD
PERPENDICULAR	E.J.	EXPANSION JOINT	INSUL.	INSULATION	R.W.L.	RAIN WATER LEADER
CHANNEL	EL.	ELEVATION	INT.	INTERIOR	SOUTH	SOUTH
POUND OR NUMBER	ELEC.	ELECTRICAL	JAN.	JANITOR	S.C.	SOLID CORE
EXISTING	ELEV.	ELEVATOR	JT.	JOINT	S.C.D.	SEAT COVER DISPENSER
ACOUST.	EMER.	EMERGENCY	KIT.	KITCHEN	SCHED.	SCHEDULE
A.D.	ENCL.	ENCLOSURE	LAB.	LABORATORY	S.D.	SOAP DISPENSER
ADJ.	E.P.	ELECTRICAL PANEL	LAM.	LAMINATE	SECT.	SECTION
A.F.F.	EQ.	EQUAL	LAV.	LAVATORY	SH.	SHELF
AGGR.	EQPT.	EQUIPMENT	LKR.	LOCKER	SHR.	SHOWER
AL.	ETC.	ETCETERA	LT.	LIGHT	SHT.	SHEET
ALUMINUM	E.W.C.	ELECTRIC H200 COOLER	MAX.	MAXIMUM	SHR.	SHEET
APPROX.	(E)	EXISTING	M.C.	MEDICINE CABINET	SHR.	SHEET
ARCH.	EXIST.	EXISTING	MECH.	MECHANICAL	SHR.	SHEET
ASBESTOS	EXP.O.	EXPOSED	MEMB.	MEMBRANE	SIM.	SIMILAR
ASBO	EXP.	EXPANSION	MTL.	METAL	S.N.D.	SANITARY NAPKIN DISPENSER
ASPH.	EXT.	EXTERIOR	MFR.	MANUFACTURER	S.N.R.	SANITARY NAPKIN RECEPTACLE
BD.	F.A.	FIRE ALARM	MH.	MANHOLE	SNT.	SEALANT
BITUM.	F.B.	FLAT BAR	MIN.	MINIMUM	SPEC.	SPECIFICATION
BLDG.	F.D.	FLOOR DRAIN	MIR.	MIRROR	SQ.	SQUARE
BLK.	FDN.	FOUNDATION	MISC.	MISCELLANEOUS	S.S.	STAINLESS STEEL
BLKG.	F.E.	FIRE EXTINGUISHER	M.O.	MASONRY OPENING	S.SK.	SERVICE SINK
BM.	F.E.C.	FIRE EXTINGUISHER CAB	MTD.	MOUNTED	STA.	STATION
BOT.	F.H.C.	FIRE HOSE CABINET	MUL.	MULLION	STD.	STANDARD
B.O.	FIN.	FINISH	MW	MONITORING WELL	STL.	STEEL
B.T.B.	FL.	FLOOR	N.	NEW	STOR.	STORAGE
CAB.	FLASH.	FLASHING	N.I.C.	NOT IN CONTRACT	STR.L.	STRUCTURAL
C.B.	FLUOR.	FLUORESCENT	NO. OR #	NUMBER	SUSP.	SUSPENDED
CEM.	F.O.C.	FACE OF CONCRETE	NOM.	NOMINAL	SYM.	SYMMETRICAL
CER.	F.O.F.	FACE OF FINISH	N.T.S.	NOT TO SCALE	T.B.	TREAD
C.I.	F.O.S.	FACE OF STUDS	O.A.	OVERALL	T.B.	TOWEL BAR
CLF	FPRF.	FIREPROOF	OBS.	OBSCURE	T.C.	TOP OF CURB
CLG.	F.S.	FOOT SIZE	O.C.	ON CENTER	TEL.	TELEPHONE
CLO.	FT.	FOOT OR FEET	O.D.	OUTSIDE DIAMETER (DIM.)	TER.	TERRAZZO
CLR.	F.T.	FIRE TREATED	OFF.	OFFICE	T.&G.	TONGUE AND GROOVE
CNTR.	FTG.	FOOTING	OPNG.	OPENING	THICK	THICK
COL.	FURR.	FURRING	OPP.	OPPOSITE	T.O.	TRIMED OPENING
CONC.	F&I	FINISH and INSTALL	OPST.	OPPOSITE	T.P.	TOP OF PAVEMENT
CONSTR.	FUT.	FUTURE	PL.	PLATE	T.P.D.	TOILET PAPER DISPENSER
CONT.	F.W.P.	FABRIC WRAPPED PANEL	P.LAM.	PLASTIC LAMINATE	T.V.	TELEVISION
CORR.	G.A.	GALVE	PLAS.	PLASTER	T.W.	TOP OF WALL
CTS.K.	GALV.	GALVANIZED	PLYWD.	PLYWOOD	TYP.	TYPICAL
CTR.	G.B.	GRAB BAR	PR.	PAIR	U.S.	UNDERSIDE
DBL.	G.C.	GENERAL CONTRACTOR	PT.	POINT	UNF.	UNFINISHED
DEPT.	G.L.	GLASS/GLAZING	P.T.D.	PAPER TOWEL DISPENSER	U.O.N.	UNLESS OTHERWISE NOTED
D.F.	GND.	GROUND	P.T.D/R	COMBINATION PAPER TOWEL DISPENSER & RECEPTACLE	UR.	URINAL
DET.	GR.	GRADE	Q.T.	QUARRY TILE	V.C.T.	VINYL COMPOSITION TILE
DIA.	GYP.	GYPSUM	R	RELOCATE	VERT.	VERTICAL
DIM.	GWB	GYPSUM WALL BOARD	RAD.	RADIUS	VEST.	VESTIBULE
DISP.	H.B.	HOME BID	R.D.	ROOF DRAIN	V.I.F.	VERIFY IN FIELD
DN.	H.C.	HOLLOW CAB	REF.	REFERENCE	W.	WEST
D.O.	HDWD.	HARDWOOD	REFR.	REFRIGERATOR	W/	WITH
DR.	HDWE.	HARDWARE	RGR.	REGISTER	W.C.	WATER CLOSET
DS.	H.M.	HOLLOW METAL	R.I.N.F.	REINFORCED	WD.	WOOD
DWR.					W/O	WITHOUT
					WP.	WATERPROOF
					WSCT.	WAINSCOT
					WT.	WEIGHT

## LEGEND

 SMH SANITARY MANHOLE  
 PROPOSED STORM WATER LEACHING BASIN  
 EXISTING STORM WATER LEACHING BASIN  
 LEACHING BASIN W/ OPEN GRATE CASTING  
 PB LEACHING BASIN W/ OPEN GRATE CASTING, INSTALLED WITH POLLUTION PREVENTION BAFFLE  
 1  
A201 ELEVATION NUMBER  
 3  
A407 DRAWING NUMBER  
 1  
A304 DETAIL REFERENCE NUMBER  
 1  
A304 DRAWING REFERENCE NUMBER  
 DENOTES HANDICAPPED USE  
 2A: 40 BC FIRE EXTINGUISHER  
 2B: 150 LB WHEELED FIRE EXTINGUISHER  
 3" DIA. HDPE DRAIN PIPE, INSIDE 6" DIA. HDPE CONTAINMENT PIPE  
 DW FUEL PRODUCT PIPING (JET = 3", AVGS = 2")  
 JET FUEL AND AVGS FUEL PIPING  
 DRAIN PIPE GRAVITY DRAINED FROM CONTAINMENT DIKE  
 AVGS VAPOR RECOVERY

[illegible]

NOTICE  
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PERSON, UNLESS ACTING UNDER THE  
DIRECTION OF A PROFESSIONAL  
ENGINEER, OR LICENSED ARCHITECT, TO  
ALTER THIS DRAWING

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	<p>Central Florida Office</p>	<p>1460 Breezy Way Spring Hill, FL 34608</p> <p>Phone: (352) 684-7275 Fax (800) 660-6724</p> <p>Email: alex@eryouengineering.com</p>



INSTALLATION OF A NEW  
ABOVE GROUND FUEL TANK  
LEADING EDGE AVIATION  
LOGAN-CACHE AIRPORT  
2500 NORTH AIRPORT DRIVE  
LOGAN, UTAH 81321

SHEET DESCRIPTION:

## ABBREVIATIONS, LEGEND & NOTES

SEAL & SIGNATURE	DATE: <b>DECEMBER 2019</b> PROJECT NO.: <b>AEAC-LOGAN</b> DRAWING BY: <b>MSK</b> CHK. BY: <b>AGN</b> DWG No: <b>T-120</b>
BRIAN E. LEWIS, P.E. UTAH P.E. # 5013588-2203 EXP. DATE: 3/31/2021	CADD FILE NO. <b>LOGAN-CACHE-Set.dwg</b>





CADD FILE NO.  
LOGAN-CACHE-Set.dwg

EMERGENCY PROCEDURE

IN THE EVENT OF A FUEL SPILL OR FIRE AT THE FUEL FACILITY, THE FUELERS SHALL IMPLEMENT THE WRITTEN EMERGENCY RESPONSE PLAN (REQUIRED UNDER 40CFR PART 112). AFTER THE IMMEDIATE EMERGENCY IS RESPONDED TO BY ACTIVATING THE EMERGENCY STOPS, THE AGENCIES LISTED IN THE ERP & SPCC PLANS SHALL BE CONTACTED PROMPTLY. THE FUELING FACILITY OWNER / OPERATOR SHALL HOLD ERP & SPCC TRAINING EXERCISES ANNUALLY TO ENSURE THAT THE FUELING SYSTEM OPERATORS ARE FULLY AWARE OF THE PROCEDURES AND CHAIN OF COMMAND FOR OUTSIDE AGENCIES.

EMERGENCY PROCEDURE SIGN

NTS

HEALTH HAZARD

FLAMMABLE

1

2

0

SPECIFIC HAZARD

REACTIVITY

NOT TO SCALE

NFPA DIAMOND DESCRIPTION

NOT TO SCALE

RED BACKGROUND

6" BLACK NUMBERS TYPICAL

15" TYPICAL

15" TYPICAL

1

2

0

BLUE BACKGROUND

1/2" BLACK BORDER

WHITE BACKGROUND

YELLOW BACKGROUND

1/2" BLACK DIVIDERS

NOT TO SCALE

N.F.P.A. LABELS

NOT TO SCALE

N.F.P.A. LABEL (VARIES)

CONFINED SPACE

MATERIAL IDENTIFICATION

TANK NUMBER DISPLAY WHEN APPLICABLE

MAXIMUM FILL LEVEL

NOTE: LOCATE SIGN ADJACENT TO LEVEL GAUGE

NOT TO SCALE

HORIZONTAL TANK - END VIEW

NOT TO SCALE

24" TYPICAL

18" TYPICAL

MAXIMUM FILL LEVEL

---,--- GALLONS

(---FT. ---")

10" TYPICAL

14" TYPICAL

DANGER

CONFINED SPACE

ENTER BY PERMIT ONLY

NOT TO SCALE

CONFINED SPACE & MAX FILL SIGNAGE DETAILS

NOT TO SCALE

E-STOP SIGNAGE NOTES

1. EMERGENCY SHUT OFF TO BE MANUALLY RESETTABLE AND DESIGNED TO ALLOW ONLY AUTHORIZED PERSONNEL TO RESET THE SYSTEM.

2. SIGNS FOR EMERGENCY SHUT OFF MUST BE INSTALLED 7 FT ABOVE FINISHED GRADE WITHIN 50 FT, IN ACCORDANCE WITH NFPA 704.

3. LETTERS MUST BE MINIMUM 2" TALL.

16"

14"

EMERGENCY SHUT-OFF SWITCH

ANSI Z535 and OSHA 1910.145 APPROVED  
TO BE INSTALLED ABOVE EMERGENCY SHUT-OFF  
IN CLEAR VIEW OF DISPENSERS

NOT TO SCALE

EMERGENCY FUEL SHUT-OFF PUSH BUTTON

NOT TO SCALE

SIGNAGE NOTES

1. ALL SIGNAGE TO BE INSTALLED AS PER AIRPORT REQUIREMENTS.

2. ALL TANKS TO BE LABELED AND PIPING TO BE COLOR CODED AND LABELED AS REQUIRED, BY FAA & API.

FUEL AREA WARNING SIGNAGE & LABELING SPECS

FUELING AREAS SHALL BE AFFIXED WITH HAZARD IDENTIFICATION SIGNAGE AS PER IFC 2018.

HAZARD IDENTIFICATION SIGNS: UNLESS OTHERWISE EXEMPTED BY THE FIRE CODE OFFICIAL, VISIBLE HAZARD IDENTIFICATION SIGNS AS SPECIFIED IN NFPA 704 FOR THE SPECIFIC MATERIAL CONTAINED SHALL BE PLACED ON STATIONARY CONTAINERS AND ABOVE GROUND TANKS AT ENTRANCES TO LOCATIONS WHERE HAZARDOUS MATERIALS ARE STORED, DISPENSED, USED OR HANDLED IN QUANTITIES REQUIRING A PERMIT AND AT SPECIFIC ENTRANCES AND LOCATIONS DESIGNATED BY THE FIRE CODE OFFICIAL.

SIGNAGE SHALL NOT BE OBSCURED OR REMOVED, SHALL BE ENGLISH AS A PRIMARY LANGUAGE OR IN SYMBOLS ALLOWED BY THIS CODE, SHALL BE DURABLE, AND THE SIZE, COLOR AND LETTERING SHALL BE APPROVED.

LABELING & SIGNAGE: THE FIRE CODE OFFICIAL IS AUTHORIZED TO REQUIRE WARNING SIGNS FOR THE PURPOSES OF IDENTIFYING THE HAZARDS OF STORING OR USING FLAMMABLE LIQUIDS. SIGNAGE FOR IDENTIFICATION AND WARNING SUCH AS FOR THE INHERENT HAZARD OF FLAMMABLE LIQUIDS OR SMOKING SHALL BE PROVIDED.

SIGNAGE DESIGN REQUIREMENTS

SIGNAGE DESIGN REQUIREMENTS

INFORMATIONAL SIGNAGE IS REQUIRED FOR SOME SITE USES AND ACTIVITIES THAT HAVE THE POTENTIAL TO CONTAMINATE STORM WATER. SIGNAGE ADDRESSES GOOD HOUSEKEEPING RULES AND PROVIDES EMERGENCY RESPONSE MEASURES IN CASE OF AN ACCIDENTAL SPILL.

ANY APPLICABLE SPILL RESPONSE SUPPLIES NEED TO BE CLEARLY MARKED AND LOCATED WHERE THE SIGNAGE IS POSTED AND NEAR THE HIGH-RISK ACTIVITY AREA. MORE THAN ONE SPILL RESPONSE KIT MAY BE NECESSARY TO ACCOMMODATE LARGER ACTIVITY AREAS. SPILL RESPONSE SUPPLIES, SUCH AS ABSORBENT MATERIAL AND PROTECTIVE CLOTHING, TO BE AVAILABLE AT ALL POTENTIAL SPILL AREAS. EMPLOYEES SHOULD BE FAMILIAR WITH THE SITE'S OPERATIONS AND MAINTENANCE PLAN AND/OR PROPER SPILL CLEANUP PROCEDURES.

ALL SIGNAGE MUST CONFORM TO THE REQUIREMENTS DESCRIBED BELOW.

- SIGNS MUST BE LOCATED WHERE THEY ARE PLAINLY VISIBLE FROM ALL ACTIVITY AREAS. MORE THAN ONE SIGN MAY BE NEEDED TO ACCOMMODATE LARGER ACTIVITY AREAS.

- SIGNS MUST BE WATER-RESISTANT.

- SIGNS MUST PROVIDE SAFETY PRECAUTIONS.

- SIGNS MUST PROVIDE IMMEDIATE SPILL RESPONSE PROCEDURES--FOR EXAMPLE: "TURN THE VALVE LOCATED AT..." AND "USE ABSORBENT MATERIALS."

- SIGNS MUST HAVE EMERGENCY CONTACT(S) AND TELEPHONE NUMBER(S)--FOR EXAMPLE: "CALL 911" AND "FL DEP ORLANDO EMERGENCY RESPONSE NUMBER 407-897-4100".

REQUIREMENTS

SIGNAGE MUST BE PROVIDED AT THE STORAGE AREA AND AT SHUT-OFF VALVES IF HAZARDOUS MATERIALS OR OTHER MATERIALS OF CONCERN ARE STORED (AS DETERMINED BY BES). SIGNAGE MUST BE LOCATED SO IT IS PLAINLY VISIBLE FROM ALL STORAGE ACTIVITY AREAS AND LOCATED NEXT TO THE SHUT-OFF VALVE. MORE THAN ONE SIGN MAY BE NEEDED TO ACCOMMODATE LARGE STORAGE AREAS.

REQUIREMENTS

SIGNAGE MUST BE PROVIDED AT THE FUEL DISPENSING AREA AND MUST BE PLAINLY VISIBLE FROM ALL FUELING ACTIVITY AREAS. SIGNAGE MUST ALSO BE PROVIDED AT THE SHUT-OFF VALVE AREAS.

ISSUES/REVISIONS

Table with 3 columns: No., Description, Date

NOTICE

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LE AVIATION

LEADING EDGE AVIATION  
2500 NORTH AIRPORT DRIVE  
LOGAN, UTAH 81321

American Environmental Aviation

3977 AVIATION LOOP, SANFORD, FLORIDA 32773  
PHONE: (631) 586-2000

INSTALLATION OF A NEW ABOVE GROND FUEL TANK

LEADING EDGE AVIATION  
LOGAN-CACHE AIRPORT  
2500 NORTH AIRPORT DRIVE  
LOGAN, UTAH 81321

SHEET DESCRIPTION:

SIGNAGE & TANK LABELING DETAILS & SPECIFICATIONS

SEAL & SIGNATURE

DATE: DECEMBER 2019

PROJECT NO.: AEAC-LOGAN

DRAWING BY: MSK

CHK. BY: AGN

DWG No:

D-200

BRIAN E. LEWIS, P.E.  
UTAH P.E. # 5013586-2203  
EXP. DATE: 3/31/2021

CADD FILE NO.  
LOGAN-CACHE-Set.dwg



## Model 178 Threaded Style Fill Cap

The 178 Series Tight Seal Threaded Style Fill Cap is used for the fill port of small storage tanks. It is lockable with a padlock.

The 178DT is made for a drop tube with 2" straight pipe threads and Buna-N gasket. The 178GSP is labeled "Gauge Stick Port."

The 178 Iron Caps are powder coated for a more durable finish.

LD. NUMBER	A	B	C	Weight
178-0100-AC	2"	13"	IR	2
178-0200-AC	2"	13"	BR	2
178-0300-AC	2"	ER	IR	2
178-0400-AC	2"	ER	BR	2.25
178-0500-AC	2 1/2"	ER	IR	2.25
178-0600-AC	3"	12"	IR	4.0
178-1000-AC	4"	13"	IR	4.5
178-1100-AC	4"	13"	IR	4.5
178-1100-AC	4"	ER	IR	4.5
178-1200-AC	4"	13"	IR	6.75
178-1300-AC	4"	13"	BR	7
178-1500-AC	4"	ER	IR	7
178-1600-AC	4"	ER	BR	7
178-1800-AC	4"	ER	IR	2
178-1900-AC	2"	ER	AL	1

**SPECIFICATION OPTIONS:**  
A—Size  
B—Body Flange (FF), Iron (FI)  
C—Cap Brass (BR), Iron (IR), AL (Aluminum)  
Weight—Shipping Weight

570 E. 7th Street, P.O. Box 238 | Dubuque, IA 52004-0238  
t: 563.583.5701 | f: 563.583.4840 | t: 563.583.5028  
www.morrisonbros.com

## 2" GAUGE HATCH (MORRISON 178-0400-AC)

NOT TO SCALE

## Model 334 Series Single Poppet Foot Valve & Model 335A Double Poppet Foot Valve

Installed at the inlet of a suction line normally at the bottom of the storage tank as a prime holding valve.

### Features

- Metal to metal seat for long life
- Screen helps filter debris from collecting on the seats

### Construction Details

- Brass body and poppet
- 20 mesh stainless steel screen
- 335A**—Dual seats provide twice the prime holding capability as single poppet designs

LD. Number	A	OD	Height	Weight
334-0100-AV	1"	1.80"	3.37"	1
334-0200-AV	1"	2.04"	4.16"	1.5
334-0300-AV	1 1/2"	2.40"	4.16"	2.25
334-0400-AV	2"	2.60"	5.16"	4.15
334BP-0100-AV	1 1/2"	2.78"	3.38"	2.5
335A-0100-AV	1 1/2"	2.75"	4.16"	4.5
335A-0200-AV	2"	3.00"	5.16"	6.5

**SPECIFICATION OPTIONS:**  
A—Thread size  
OD—Outside diameter of valve  
Height—Height of valve  
Weight—Shipping weight in lbs.

570 E. 7th Street, P.O. Box 238 | Dubuque, IA 52004-0238  
t: 563.583.5701 | f: 563.583.4840 | t: 563.583.5028  
www.morrisonbros.com

## 2" FOOT VALVE (MORRISON 334-0400-AV)

NOT TO SCALE

## EMERGENCY VENTS

**New! LISTED!**



367



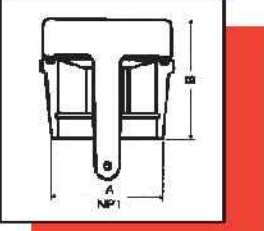
368

### Features

Stimline style is ideal for limited space applications. Heavy duty cast iron top and base with aluminum bases available. Machined top and base with a Buna N "O" Ring provide a vapor tight seal. 1" or 6" male NPT thread standard. Screen minimizes chances of theft or vandalism.

Airflow:  
w/o screen      w/o screen  
4" - 91,076 SCFH      105,160 SCFH  
5" - 171,300 SCFH      194,725 SCFH  
6" - 227,191 SCFH      267,281 SCFH

Part No.      Size      Wt. lbs.      B.  
A NPT  
0367-01-6000 4" 17.0 4 1 1/8"  
0367-01-5000 5" 22.0 5"  
0367-01-6000 6" 28.0 5 1/8"  
0367-03-6000 4" 14.0 4 1 1/8"  
0367-03-5000 5" 20.0 5"  
0367-03-6000 6" 25.0 5 1/8"  
\*Aluminum Base

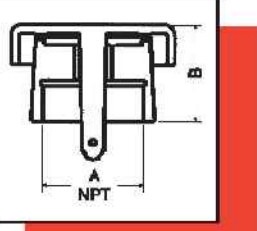


### Features

Female thread, hood design made of heavy duty cast iron top and base with aluminum bases available. Hood design acts as a weather deflector and minimizes chances of theft or vandalism. Machined top & base with Buna N "O" Ring provides a vapor tight seal.

Airflow:  
w/o screen      w/o screen  
4" - 77,415 SCFH      89,611 SCFH  
5" - 210,527 SCFH      227,988 SCFH  
6" - 177,033 SCFH      253,507 SCFH

Part No.      Size      Wt. lbs.      B.  
A NPT  
0368-01-6000 4" 21.0 4 1/8"  
0368-01-6000 6" 31.0 5"  
0368-01-6000 6" 41.0 4 1/8"  
0368-03-6000 4" 19.0 4 1/8"  
0368-03-6000 6" 28.0 5"  
0368-03-6000 6" 37.0 4 1/8"  
\*Aluminum Base



## 6" EMERGENCY VENT (CLAY BAILEY 0367-01-6000)

NOT TO SCALE

## Morrison Bros. Co. Fig. 749 Pressure Vacuum Vents Specification Sheet

The Fig. 749 Vent valve is used on underground & low volume aboveground tanks for motor fueling. The vent allows the tank to "breathe" during filling & dispensing operations. Poppet seal vapor in the tank when pressure is equalized. Settings are approximate. Fig. 749C RB5000 and 749C RB5000 offer C.A.R.B. approval for Stage I vapor recovery systems. Fig. 749CR B6000 and 749CR B5000 offer C.A.R.B. approval for Stage I & II vapor recovery systems.

**WARNING:** Fig. 749 pressure vacuum vents must only be used in connection with motor fueling and/or low capacity flow. Fuel handling in their larger than that rated for in full scale status can cause tank to rupture or explode.

**WARNING:** DO NOT FILL OR UNLOAD FUEL FROM A STORAGE TANK UNLESS IT IS CERTAIN THAT THE TANK VENTS WILL OPERATE PROPERLY. Moreover, they have been designed only for use on shop bonded atmospheric tanks which have been designed and tested in accordance with UL 142, NFPA 30 & 30A, and API 650 and in accordance with all applicable local, state, and federal laws. In normal operation, vented vapor can accumulate in vent openings and block air passages. Certain atmospheric conditions such as a sudden drop in temperature, or low enough temperature, and the venting can cause moisture to enter the vent and freeze which can restrict the normal movement of vent mechanisms and block air passages. All blockage tank vent air passages must be completely free of restriction and all vent mechanisms must have the movement in order to have proper operation. Any restriction of airflow can cause excessive pressure or vacuum to build up in the storage tank, which can result in structural damage to the tank, and possibly, property damage, fire, injury, and death. Monthly inspection, and immediate inspection during freezing conditions, by someone familiar with the proper operation of blockage tank units, is required to have venting devices are functioning properly before filling or unloading a tank.

### SPECIFICATION OPTIONS:

LD. NUMBER	A	B	C	D	E	F	G	H	WT	SCFH
749-0100-AV	2"	32"	1/2"	M	N	4.30	1	1500	0.25 oz./sq. in.	25
749CRB5000-AV	2"	32"	1/2"	M	N	4.30	1.45	1500	0.25 oz./sq. in.	25
749-0200-AV	2 1/2"	122"	1/2"	M	N	4.30	1	1500	0.25 oz./sq. in.	25
749CRB5000-AV	2 1/2"	122"	1/2"	M	N	4.30	1.45	1500	0.25 oz./sq. in.	25
749-0300-AV	2 1/2"	122"	1/2"	M	N	4.30	1	1500	0.25 oz./sq. in.	25
749CRB5000-AV	2 1/2"	122"	1/2"	M	N	4.30	1.45	1500	0.25 oz./sq. in.	25
749-0400-AV	2 1/2"	122"	1/2"	M	N	4.30	1	1500	0.25 oz./sq. in.	25
749CRB5000-AV	2 1/2"	122"	1/2"	M	N	4.30	1.45	1500	0.25 oz./sq. in.	25
749-0500-AV	2 1/2"	122"	1/2"	M	N	4.30	1	1500	0.25 oz./sq. in.	25
749CRB5000-AV	2 1/2"	122"	1/2"	M	N	4.30	1.45	1500	0.25 oz./sq. in.	25
749-0600-AV	2 1/2"	122"	1/2"	M	N	4.30	1	1500	0.25 oz./sq. in.	25
749CRB5000-AV	2 1/2"	122"	1/2"	M	N	4.30	1.45	1500	0.25 oz./sq. in.	25

Fig. 749CR B6000 & 749CR B5000 are C.A.R.B. approved for Stage I Vapor Recovery Systems

Fig. 749CR B6000 & 749CR B5000 are C.A.R.B. approved for Stage I & II Vapor Recovery Systems

Fig. 749BP50100 and 749BP50200 have British Threads

**Morrison Bros. Co.**  
Established 1855

P.O. Box 238 • Dubuque, Iowa 52004-0238  
563.583.5701 (tel) • 800.563.4840 • 563.583.5028 (fax)  
www.morrisonbros.com

## 2" PRESSURE VACUUM VENT (MORRISON 749-0100-AV)

NOT TO SCALE

## Model 323 & 323C Series

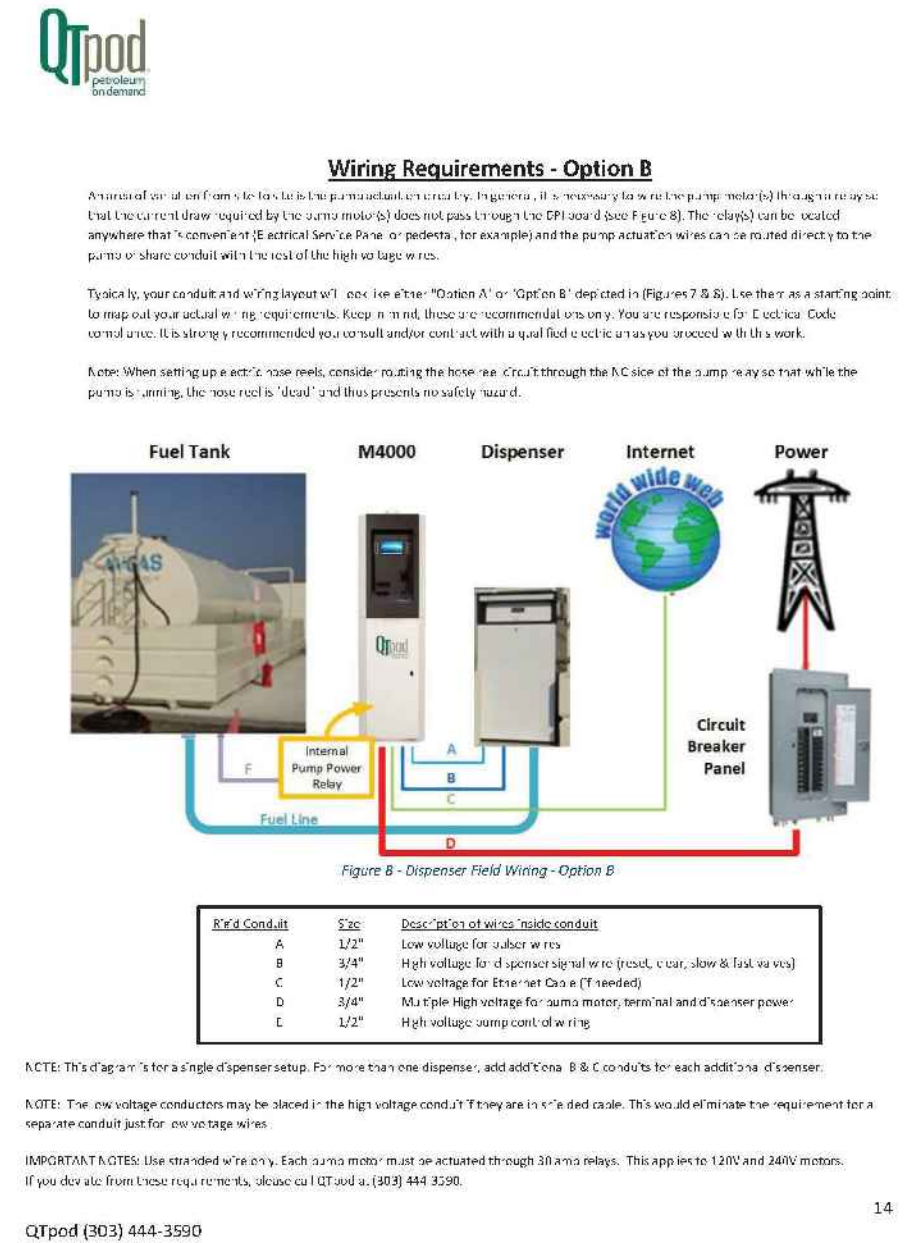
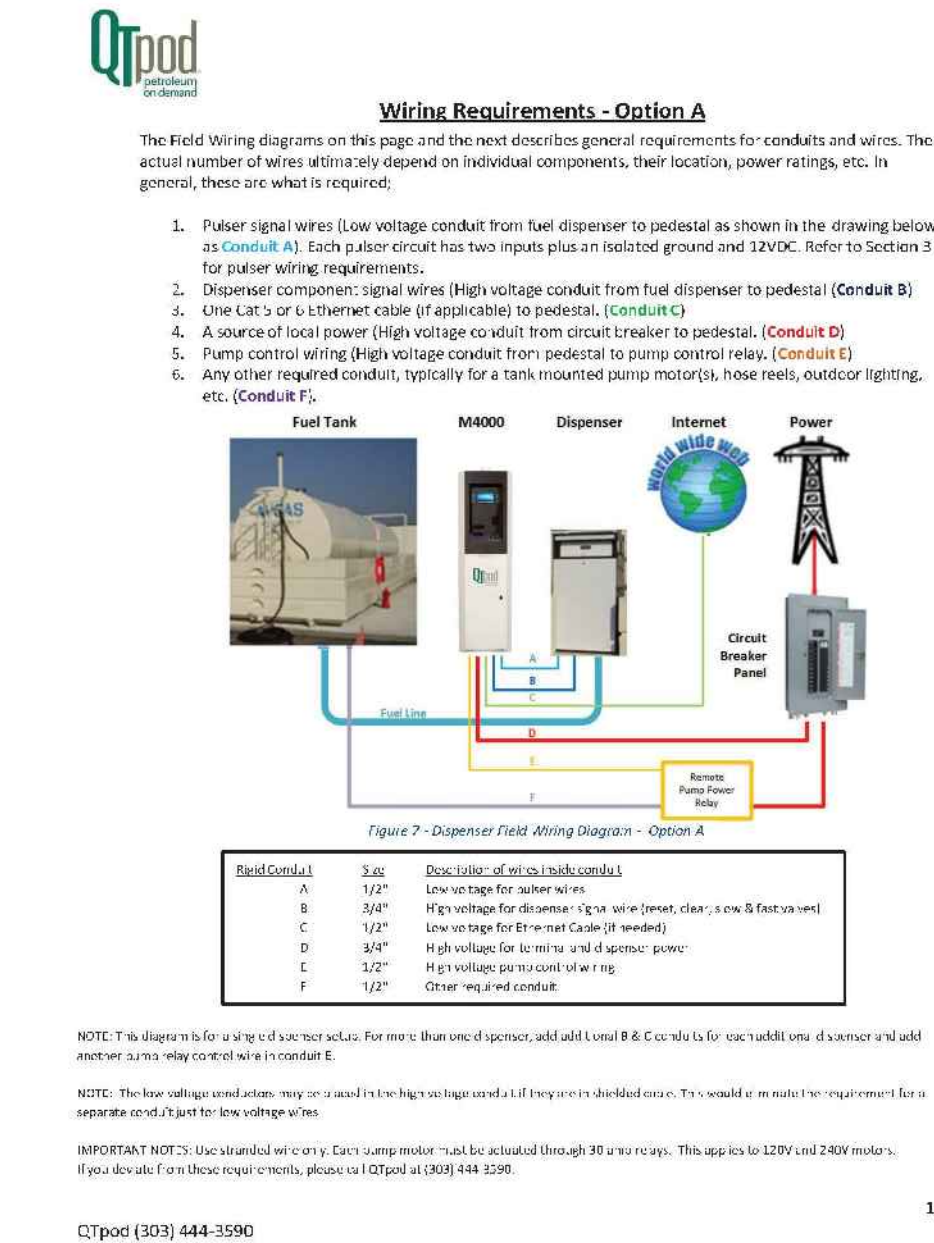
The Morrison 323 Series Adaptors are designed for installation on aboveground and underground storage tanks to mate with vapor recovery couplers & elbows. This connection allows the fuel vapors to be returned to the tank truck during the fuel delivery operation. The 323SA model has a built-in swirl. The 323B model is equipped to accept brake interlock devices which lock the transport brakes while connected.

The Morrison 323C Caps provide a lockable vapor tight cover which protects the adaptor from dirt and debris.

LD. Number	Description	A	B	C	D	E	F	G	ODH	Weight
323-0100-AA	Vapor Recovery Adaptor 1/2" Vapor, 3" Pipe Thread	3/4"	3/4"	4"	B	A	N	N	5.64	3.54
323-0200-AA	Vapor Recovery Adaptor 1/2" Vapor, 3" Pipe Thread	1/2"	3"	1"	B	A	N	N	5.68	4
323-0300-AA	Vapor Recovery Adaptor 1/2" Vapor, 3" Pipe Thread	3/8"	3"	1"	V	A	N	N	5.63	3.34
323-0300-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 3" Pipe Thread EVR	3/8"	3"	4"	V	A	N	N	5.68	3.5
323-0400-AA	Vapor Recovery Adaptor 1/2" Vapor, 4" Pipe Thread	1/2"	3"	4"	V	A	N	N	5.64	4
323-0400-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 4" Pipe Thread EVR	1/2"	3"	4"	V	A	N	N	5.64	4
323BP0100-AA	Vapor Recovery Adaptor 1/2" Vapor, 4" Pipe Thread BSP 1/2" Nuts	1/2"	3"	4"	V	A	N	N	5.68	3.54
323BP0100-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 4" Pipe Thread EVR 1/2" Nuts	1/2"	3"	4"	V	A	N	N	5.68	4
323B-0600-AA	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread EVR	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AA	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread EVR	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AA	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread EVR	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AA	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread EVR	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AA	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread EVR	1/2"	3"	4"	V	A	N	N	5.63	4
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323B-0600-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread EVR	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AA	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread	1/2"	3"	4"	V	A	N	N	5.63	4
323B-0600-AEVR	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread EVR	1/2"	3"	4"	V	A	N	N	5.63	4
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323B-0600-AA	Vapor Recovery Adaptor 1/2" Vapor, 6" Pipe Thread	1/2"	3"	4"	V	A	N	N		







NOTICE  
IT IS A VIOLATION OF LAW FOR ANY  
PERSON, UNLESS ACTING UNDER THE  
DIRECTION OF A PROFESSIONAL  
ENGINEER, OR LICENSED ARCHITECT, TO  
ALTER THIS DRAWING



**LE AVIATION**

LEADING EDGE AVIATION  
2500 NORTH AIRPORT DRIVE  
LOGAN, UTAH 81321

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American  
**Environmental**  
Aviation



3977 AVIATION LOOP, SANFORD, FLORIDA 32773  
PHONE: (631) 586-2000

INSTALLATION OF A NEW  
ABOVE GROUND FUEL TANK  
LEADING EDGE AVIATION  
LOGAN-CACHE AIRPORT  
2500 NORTH AIRPORT DRIVE  
LOGAN, UTAH 81321

## FUELING SYSTEM EQUIPMENT SPECIFICATIONS

SEAL & SIGNATURE	DATE: <b>DECEMBER 2019</b>
	PROJECT NO.: <b>AEAC-LOGAN</b>
	DRAWING BY: <b>MSK</b>
	CHK. BY: <b>AGN</b>
	DWG No:
	<b>M-230</b>
BRIAN E. LEWIS, P.E. UTAH P.E. # 5013586-2203 EXP. DATE: 3/31/2021	CADD FILE NO. <b>LOGAN-CACHE-Set.dwg</b>

NOT TO SCALE



CONTRACTOR SHALL PROTECT AND SUPPORT ALL EXISTING STRUCTURES AND EQUIPMENT ADJACENT TO THE WORK AND PROJECT, SUPPORT AND RELOCATE, IF NECESSARY, ALL EXPOSED LINES AND MAKE COMPLETE RESTORATION OF DAMAGED PIPING, CONDUITS, WIRING, CABLES AND APPURTENANCES AT NO COST TO THE OWNER OF SAID UTILITIES, AUTHORITY OR ENGINEERS.

- (B) LOCAL BUILDING AND FIRE CODES
- (C) ALL APPLICABLE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA) CODES
- (D) ALL APPLICABLE BUILDING OFFICIAL & CODE ADMINISTRATORS. (BOCA) CODES
- (E) ALL LOCAL, STATE, AND FEDERAL SAFETY REGULATIONS.
- (F) ALL LOCAL, STATE, AND FEDERAL ENVIRONMENTAL PROTECTION. REGULATIONS.
- (G) DESIGN SAFETY STANDARDS FOR ELECTRICAL SYSTEMS (OSHA)
- (H) ANY OTHER PUBLIC AGENCIES HAVING JURISDICTION.

THE DRAWINGS DO NOT INDICATE ALL OF THE EXISTING EQUIPMENT, DEVICES, WIRING, STRUCTURES, PIPING, ETC., EITHER EXPOSED OR CONCEALED. PRIOR TO COMMENCING CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN OR VERIFY THE EXACT LOCATION OF ALL THE EXISTING ITEMS THAT AFFECT THE WORK.

ALL WORK SHALL BE GROUNDED AS INDICATED ON THE DRAWINGS AND/OR AS REQUIRED IN ACCORDANCE WITH NATIONAL ELECTRICAL CODES.

THE CONTRACTOR SHALL PROVIDE EXPLOSION PROOF ELECTRICAL WIRING, EQUIPMENT AND DEVICES SUITABLE FOR INSTALLATION IN CLASS 1, DIVISION 1, GROUP C AND D LOCATIONS FOR AS REQUIRED BY NEC SECTIONS 501 AND 514 FOR THIS TYPE OF INSTALLATION.

THE CONTRACTOR SHALL PROVIDE SEAL-OFF FITTINGS AS SHOWN ON THE DRAWINGS, AND WHERE CONDUITS EGRESS OR INGRESS CLASS 1, DIVISION 1 AREAS, AND/OR AS REQUIRED BY NEC SECTIONS 501 AND 514.

	KEY RESET CONTROL		EMERGENCY PUSHBUTTON STATION
	TRANSFORMER		REMOTE ALARM
	FUSE		JUNCTION BOX
	CONTACT NO		MOTOR/HP
	CONTACT NC		SWITCH
	CIRCUIT BREAKER		FIRE ALARM SMOKE DETECTOR
	PUSH BUTTON		FIRE ALARM HORN/STROBE, 75 CANDELA, MTD. AT 80' AFF
	DEADMAN SWITCH		
	POWER PULL BOX		
	COMMUNICATIONS PULL BOX		
	FIRE ALARM PULL BOX		
	FIRE SUPPRESSION SYSTEM PULL BOX		
	ELECTRICAL PANEL		
	DISCONNECT SWITCH		
	20A, 120V, GROUNDING TYPE DUPLEX RECEPTACLE WITH GROUND FAULT PROTECTION		
	20A, 208V, GROUNDING TYPE DUPLEX RECEPTACLE		
	WALL MOUNTED LIGHTING FIXTURE		
	1X4' CEILING MOUNTED LIGHTING FIXTURE		
	EMERGENCY BATTERY PACK LIGHTING FIXTURE		
	CEILING MOUNTED JUNCTION BOX		
	LIGHTING SWITCH		
	CONDUIT RUNNING EXPOSED IN CEILING OR WALLS		

1. POWER AND CONTROL WIRES SHALL BE RUN IN SEPARATE CONDUITS.
2. ALL WIRING TO BE XHHW-2 INSULATED AND GROUNDING AS PER NEC AND NFPA 514.
3. ALL CONDUITS EXITING THE HAZARDOUS AREAS ARE TO BE SEALED WITH EPOXY FILLED SEALING FITTINGS PRIOR TO ENTRY INTO A NON-HAZARDOUS ZONES.
4. SEAL FITTINGS ARE REQUIRED AT EACH SENSOR BUT ARE NOT SHOWN FOR SIMPLICITY.
5. ALL CONDUITS TO MEET LATEST REQUIREMENTS OF NATIONAL ELECTRIC CODE.
6. (NFPA 70) AND THE AUTOMOTIVE AND MARINE SERVICE STATION CODE (NFPA 30A).
7. FOR EXACT AMOUNT AND SIZE OF LOW VOLTAGE CONTROL WIRES REFER TO MANUFACTURER'S RECOMMENDATIONS.
8. UNDERGROUND CONDUITS RUNNING FROM ELECTRICAL PANEL OR CONTROL EQUIPMENT TO DISPENSERS TO BE THREADED RIGID METAL.
9. ALL UNDERGROUND CONDUITS SHALL BE ENCASED IN CONCRETE.
10. EMERGENCY DISCONNECT SWITCHES.  
AN APPROVED, CLEARLY IDENTIFIED AND READILY ACCESSIBLE EMERGENCY DISCONNECT SWITCH SHALL BE PROVIDED AT AN APPROVED LOCATION, TO IMMEDIATELY SHUT DOWN THE TRANSFER OF FUEL TO THE FUEL DISPENSERS IN THE EVENT OF A FUEL SPILL OR OTHER EMERGENCY. AN EMERGENCY DISCONNECT SWITCH FOR EXTERIOR FUEL DISPENSERS SHALL BE LOCATED WITHIN 100 FEET 30480 MM OF, BUT NOT LESS THAN 20 FEET (6096 MM) FROM, THE FUEL DISPENSERS. FOR INTERIOR FUEL-DISPENSING OPERATIONS, THE EMERGENCY DISCONNECT SWITCH SHALL BE INSTALLED AT AN APPROVED LOCATION. AN APPROVED SIGN SHALL BE POSTED ON OR IMMEDIATELY ADJACENT TO SUCH DEVICES AND SHALL READ: EMERGENCY FUEL SHUTOFF. SUCH EMERGENCY DISCONNECT SWITCHES SHALL BE OF A TYPE THAT IS RESET MANUALLY
11. AT ALL TIMES THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITION OF JOB SITE, INCLUDING THE SAFETY OF ALL PERSONS AND PROPERTY AND FOR ALL NECESSARY INDEPENDENT ENGINEERING REVIEWS OF THESE CONDITIONS. THE ENGINEERS JOB SITE REVIEW IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTORS SAFETY MEASURES.
12. THE CONTRACTOR SHALL MAKE AN EXAMINATION OF THE SITE. HE SHALL COMPARE THE SITE WITH THE DRAWINGS AND SPECIFICATIONS AND SATISFY HIMSELF AS TO CONDITIONS UNDER WHICH WORK IS TO BE PERFORMED. HE SHALL ASCERTAIN AND CHECK THE LOCATIONS OF ANY EXISTING STRUCTURES OR EQUIPMENT WHICH MAY AFFECT THIS WORK. NO ALLOWANCE SHALL SUBSEQUENTLY BE MADE IN HIS BEHALF FOR ANY EXPENSE TO WHICH HE MAY BE PUT DUE TO FAILURE OR NEGLECT ON HIS PART TO MAKE SUCH EXAMINATION.
13. ALL WORK SHALL BE COORDINATED WITH THE OWNER TO MAINTAIN CONTINUITY OF SERVICE AND MAXIMUM UTILIZATION OF THE OWNERS FACILITY. ALL WORK SHALL BE BID ON A "NORMAL TIME" BASIS WITH PREMIUM TIME IN ADDITION ONLY AS AUTHORIZED FOR CORE BORING OR OTHER WORK WHICH WILL BE NOISY, DIRTY OR OTHERWISE OBSTRUCT THE WORK PROCESS.
14. THE CURRENT ISSUE OF ALL NFPA, CEC, CBC, UBC, UFC, ANSI, OSHA, ASTM, NEMA, AND OTHER NATIONALLY PUBLISHED CODES OR STANDARDS, AS WELL AS STATE AND LOCAL CODES AND ORDINANCES, SHALL APPLY TO THIS WORK WHETHER ADOPTED BY LOCAL AGENCIES OR NOT. THE MOST STRINGENT CODE SHALL APPLY.
15. NOTHING ON THE DRAWINGS OR SPECIFICATIONS INTENDED TO ALLOW A VIOLATION OF ELECTRICAL WORKING SPACE AROUND ELECTRICAL EQUIPMENT. A 30"W MIN x 48"D x 6'-8"H SPACE SHALL BE CLEAR TO THE FLOOR IN FRONT OF ALL ELECTRICAL PANELS, CONTROLS OR ITEMS THAT REQUIRE MAINTENANCE OR ACCESS WHILE ENERGIZED. ANY DEVIATION FROM THIS MINIMUM SHALL BE APPROVED IN WRITING.
16. ALL CONDUITS SHALL RUN TIGHT TO SLAB AND BEAMS. WHERE EQUIPMENT IS INSTALLED TIGHT TO SLAB, RUN CONDUIT BELOW OR ABOVE AS TIGHT TO EQUIPMENT AS POSSIBLE.
17. DO NOT SUPPORT CONDUITS FROM DUCTS, MECHANICAL SUPPORTS OR EQUIPMENT OF ANY KIND.
18. ALL CABLES, CONDUITS, PIPING OR EQUIPMENT LOCATIONS AND ELEVATIONS ARE APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE FOR FIELD CHECKING AND MAKING ALL NECESSARY OFFSETS, AS REQUIRED, TO AVOID EXISTING INTERFERENCES AND COORDINATE WITH OTHER TRADES.
19. IDENTIFY EACH CONDUCTOR BY SHRINK-ON INDIVIDUALLY MARKED BRADY-TAGS AND EACH ELECTRICAL ITEM BY BLACK-WHITE-BLACK ENGRAVED SCREW-ON PLASTIC NAMEPLATE, LEGEND PER DRAWING.
20. ALL GROUND WIRES SHALL BE SEGREGATED FROM PHASE CONDUCTORS IN CONDUITS TO MINIMIZE GROUND LOOPS.
21. ELECTRICAL DESIGN BASED UPON TYPICAL VENDOR EQUIPMENT. COORDINATE FINAL INSTALLATION WITH ACTUAL EQUIPMENT FURNISHED.
22. THE CONTRACTOR SHALL PROVIDE ALL FUSES REQUIRED FOR PROJECT POWER INCLUDING ANY FUSES BLOWN DURING INITIAL TESTING.
23. BONDING JUMPERS SHALL BE INSTALLED TO INSURE CONTINUITY WHERE CONDUIT CONNECTIONS AT CONCENTRIC KNOCKOUTS ARE TO SERVE AS A GROUND.
24. ELECTRICAL CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES TO ALL WALLS, FLOORS AND PAVING, IF DAMAGE OCCURS DURING CONSTRUCTION. THEY SHALL COORDINATE WITH OWNER TO PATCH, PAINT AND REPAIR TO MATCH EXISTING CONDITIONS.
25. ABOVEGROUND CONDUIT SHALL BE RIGID STEEL.
26. UNDERGROUND CONDUIT SHALL BE PVC VIRGIS WISERS EXCEPT WHERE ENTERING PANEL OR SWITCHGEAR. PROVIDE CONDUIT SEAL-OFFS AS REQUIRED BY CODE.
27. MINIMUM BURIAL DEPTH FOR UNDERGROUND CONDUITS SHALL BE 24".

[illegible]

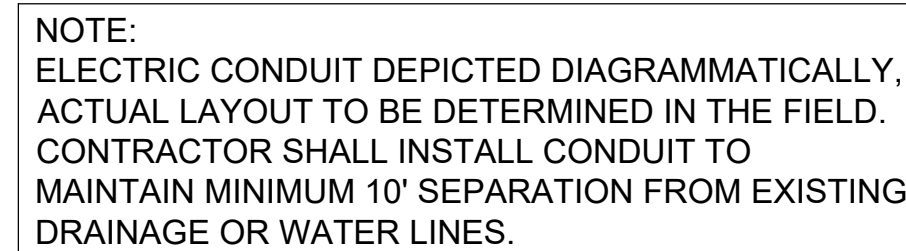
<p><b>N. D. Eryou, PhD, PE</b>  <b>Consulting Engineer</b></p>	<p>Southwest Florida Office</p>	<p><b>5051 Castello Drive, Suite 244</b>  <b>Naples, Florida 34103</b></p> <p><b>Phone: (352) 684-7275</b>  <b>Fax (800) 660-6724</b></p> <p><b>Email: alex@eryouengineering.com</b></p>
	<p>Central Florida Office</p>	<p><b>1460 Breezy Way</b>  <b>Spring Hill, FL 34608</b></p> <p><b>Phone: (352) 684-7275</b>  <b>Fax (800) 660-6724</b></p> <p><b>Email: alex@eryouengineering.com</b></p>



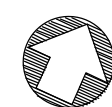
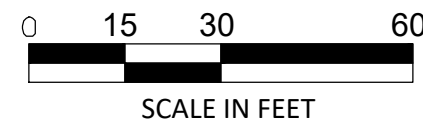
INSTALLATION OF A NEW  
ABOVE GROUND FUEL TANK  
LEADING EDGE AVIATION  
LOGAN-CACHE AIRPORT  
2500 NORTH AIRPORT DRIVE  
LOGAN, UTAH 81321

## ELECTRICAL NOTES

SEAL & SIGNATURE	DATE: DECEMBER 2019
	PROJECT NO.: AEAC-LOGAN
	DRAWING BY: MSK
	CHK. BY: AGN
	DWG No:
	E-100
BRIAN E. LEWIS, P.E. UTAH P.E. # 5013586-2203 EXP. DATE: 3/31/2021	CADD FILE NO. LOGAN-CACHE-Set.dwg



OVERALL ELECTRICAL SITE PLAN



1. FOR ELECTRICAL ABBREVIATIONS, LEGEND, NOTES & SPECIFICATIONS, SEE DWG E-100.
2. CONDUIT ROUTING IS SHOWN DIAGRAMMATICALLY ONLY AND IS NOT INTENDING TO SHOW EXACT EQUIPMENT LOCATION OR CONDUIT ROUTING. THE ELECTRICAL CONTRACTOR SHALL DETERMINE, IN THE FIELD, THE BEST ROUTING TO AVOID ANY INTERFERENCE WITH EXISTING UNDERGROUND UTILITIES, EXISTING ABOVEGROUND STRUCTURES OR OTHER EQUIPMENT. THE ELECTRICAL CONTRACTOR SHALL FOLLOW EQUIPMENT MANUFACTURER'S RECOMMENDATIONS FOR INSTALLATION AND TERMINATION OF CONDUIT AND WIRING.
3. GROUND CONNECTORS AT MAIN GROUND LOOP SHALL BE MADE USING EXOTHERMIC CONNECTIONS, SIMILAR TO "CADWELD" OR APPROVED EQUAL.
4. THE NEW GROUND LOOP CABLE SHALL BE CONTINUOUS.
5. GROUNDING CABLE SHALL BE INSTALLED WITHOUT SHARP BENDS OR KINKS, AND WHERE BENDS OR LOOPS ARE REQUIRED, THEY SHALL BE MADE WITH AS LARGE A RADIUS AS POSSIBLE.
6. ALL CONNECTIONS TO BE GROUNDED, CONNECTED OR BONDED MUST BE MADE TO CLEAN AND BRIGHT METAL SURFACES.
7. ELECTRICAL CONTRACTOR SHALL VERIFY GROUND CONNECTIONS POINTS ON COMPRESSOR AND DRYER SKIDS.
8. BURIED GROUND WIRE SHALL BE A MINIMUM OF 24 INCHES BELOW FINISHED GRADE.
9. GROUNDING SHALL BE PERFORMED AS SPECIFIED IN THE 2008 NEC, ARTICLE 250.
10. GROUND WIRES THRU CONCRETE SHALL HAVE A SLEEVE THAT EXTENDS 4" ABOVE GRADE. THE SLEEVES SHALL BE SCHEDULE 40 PVC PIPE FILLED WITH GE RTV SILICONE. AFTER GROUND WIRE HAS BEEN INSTALLED.
11. #4/0 BARE COPPER GROUND WIRE SHALL BE RUN IN TRENCH WITH POWER CONDUITS AND CONTROL CONDUITS.
12. USE COUPLING AND DRIVING STUD TO DRIVE GROUND ROD TO AVOID MUSHROOMING.
13. BOND TOGETHER ALL CONDUIT STUB-UPS IN SAME LOCATION AND CONNECT TO MAIN GROUND CABLE.
14. THE NEW GROUNDING SYSTEM SHALL BE TESTED TO SHOW A RESISTANCE TO GROUND OF NO MORE THAN 25 OHMS.
15. GROUND TESTS CALLED FOR IN NOTES 14 SHALL BE CARRIED OUT USING A "DEDICATED GROUND TESTER".
16. REMOVABLE GUARD POSTS HAVE SUBSTANTIAL CONCRETE FOOTING SEE POST GROUNDING DETAIL ON THIS DRAWING. ELECTRICAL CONTRACTOR SHALL COORDINATE LOCATION OF GROUND LOOP TO AVOID THESE FOOTINGS.
17. EACH VESSEL SHALL BE GROUNDED.
18. ALL CONDUITS IN PULL BOX SHALL BE BONDED TOGETHER AND CONNECTED TO MAIN GROUND LOOP WITH #2AWG COPPER WIRE.

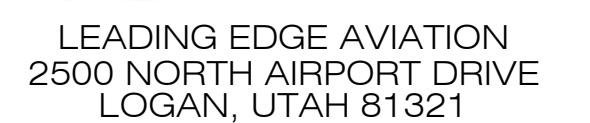
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Fax (800) 660-6724  
Email: alex@eryouengineering.com**



3977 AVIATION LOOP, SANFORD, FLORIDA 32773  
PHONE: (631) 586-2000

SHEET DESCRIPTION:

# ELECTRICAL SITE, GROUNDING & BONDING PLAN

SEAL &amp; SIGNATURE

DATE: DECEMBER 2019
PROJECT NO.: AEAC-LOGAN
DRAWING BY: MSK
CHK. BY: AGN
DWG No:

E-200

BRIAN E. LEWIS, P.E.  
UTAH P.E. # 5013586-2203  
EXP. DATE: 3/31/2021

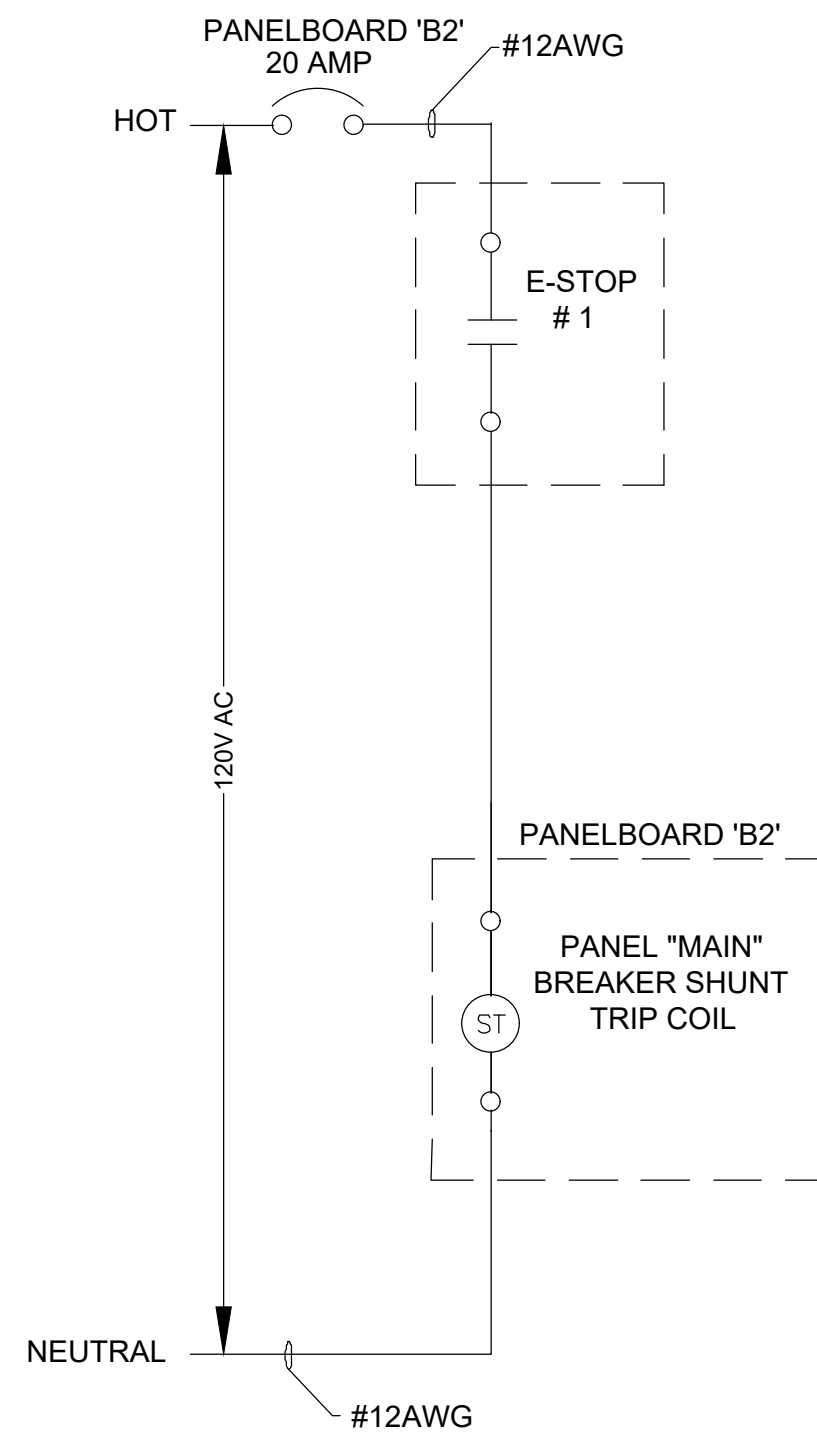
CADD FILE NO.  
LOGAN-CACHE-Set.dwg

CONNECTED		VOLTAMPS		LOAD DESCRIPTION		WIRES	POLES	AMPS	CT.	SW.	AMPS	POLES	LOAD DESCRIPTION	WIRES	CONNECTED VA
AØ	BØ								A B					AØ	BØ
		SPACE						1	•	2					
								2	•	2					
3450		FUEL FARM - ELECTRIC RACK				2	2	100	3	20-20			EXISTING LOAD		
	1200	SUB-PANEL "B2" FEEDER						5	•	8					
								7	•	8					
3450	1200	TOTAL CONNECTED VA		TOTAL CONNECTED AMPS									TOTAL DEMAND VA	SUB-TOTAL VA	
SUB-TOTAL VA													TOTAL DEMAND AMPS	SUB-TOTAL VA	

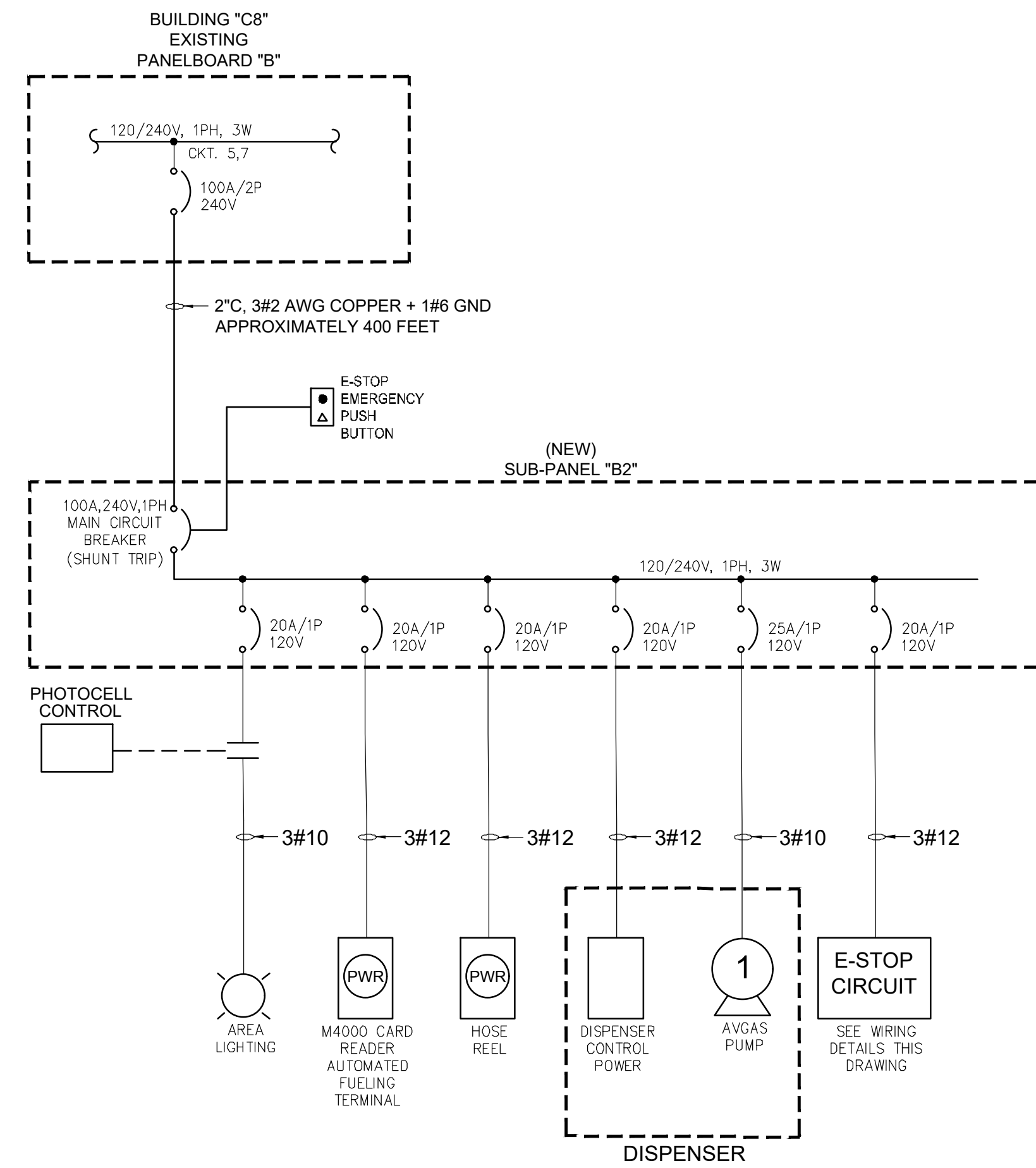
PANELBOARD RATINGS				PANELBOARD FEATURES			
VOLTAGE		BUS AMPS		MAIN DEVICE		ENCLOSURE	
<input type="checkbox"/> 120/208 3ø,4W <input type="checkbox"/> 120/240 1ø,4W <input type="checkbox"/> 277/480 3ø,4W		<input type="checkbox"/> 100A <input type="checkbox"/> 400A <input type="checkbox"/> 200A <input type="checkbox"/> 600A <input type="checkbox"/> 225A <input type="checkbox"/> ____A		<input type="checkbox"/> MAIN LUGS ONLY <input type="checkbox"/> 100A CIRCUIT BREAKER <input type="checkbox"/> ____A FUSED SWITCH		<input type="checkbox"/> FLUSH <input type="checkbox"/> SURFACE <input type="checkbox"/> NEMA 4X <input type="checkbox"/> HINGED DOOR <input type="checkbox"/> KEYED LATCH <input type="checkbox"/> DOOR-IN-DOOR <input type="checkbox"/> STAINLESS STEEL <input type="checkbox"/> PAINTED STEEL <input type="checkbox"/> NAMEPLATE	
INTERUPTING RATING (AIC SYM)				SERVICE ENTRANCE RATED		BRANCH POLES	
<input type="checkbox"/> 10,000A <input type="checkbox"/> 30,000A <input type="checkbox"/> 65,000A <input type="checkbox"/> 14,000A <input type="checkbox"/> 42,000A <input type="checkbox"/> 100,000A <input type="checkbox"/> 22,000A <input type="checkbox"/> 50,000A <input type="checkbox"/> ____ ,000A <input type="checkbox"/> SERIES RATING PERMITTED		<input type="checkbox"/> NEUTRAL BUS <input type="checkbox"/> BUS MATERIAL <input type="checkbox"/> 50% <input type="checkbox"/> 200% <input type="checkbox"/> COPPER <input type="checkbox"/> 100% <input type="checkbox"/> ____ % <input type="checkbox"/> ALUMINUM <input type="checkbox"/> GROUND BUS REQUIRED		<input type="checkbox"/> 8 <input type="checkbox"/> 36 <input type="checkbox"/> 18 <input type="checkbox"/> 42 <input type="checkbox"/> 24 <input type="checkbox"/> ____		<input type="checkbox"/> CIRCUIT BREAKERS <input type="checkbox"/> FUSES <input type="checkbox"/> ____ TO BE GFCI BREAKERS <input type="checkbox"/> LOAD CENTER PERMITTED	
<input type="checkbox"/> ____ PANELS IN PARALLEL <input type="checkbox"/> THROUGH FEED LUGS REQUIRED				NOTES ① ② ③ ④ ⑤			

CONNECTED VOLTPS		LOAD DESCRIPTION	WRES AWG	POLES	AMPS	BUS A B	TH X	AMPS	POLES	LOAD DESCRIPTION	WRES AWG	CONNECTED VA	
AØ	BØ											AØ	BØ
360		LIGHTING	12	1	20	1	2			SPACE			
	600	CARD READER - M4000	12	1	20	3	4						
1200		HOSE REEL	12	1	20	5	6						
	600	DISPENSER	12	1	20	7	8						
1920		(HP) DISPENSER PUMP MOTOR	10	1	20	9	10						
		SPACE				11	12						
						13	14						
						15	16						
						17	18						
						19	20						
						21	22						
						23	24						
3480	1200		3480	1200									
SUB-TOTAL VA		TOTAL CONNECTED VA	TOTAL CONNECTED AMPS							TOTAL DEMAND VA		SUB-TOTAL VA	
										TOTAL DEMAND AMPS			

PANELBOARD RATINGS				PANELBOARD FEATURES			
VOLTAGE		BUS AMPS		MAIN DEVICE		ENCLOSURE	
<input type="checkbox"/> 120/208 3ø,4W	<input type="checkbox"/> 100A	<input type="checkbox"/> 400A	<input type="checkbox"/> MAIN LUGS ONLY		<input type="checkbox"/> FLUSH		BRANCH POLES
<input type="checkbox"/> 120/240 1ø,4W	<input type="checkbox"/> 200A	<input type="checkbox"/> 800A	<input type="checkbox"/> 100A CIRCUIT BREAKER		<input type="checkbox"/> SURFACE		<input type="checkbox"/> CIRCUIT BREAKERS <input type="checkbox"/> FUSES
<input type="checkbox"/> 277/480 3ø,4W	<input type="checkbox"/> 225A	<input type="checkbox"/> ____A	<input type="checkbox"/> ____A FUSED SWITCH		<input type="checkbox"/> NEMA 4X		<input type="checkbox"/> ____ TO BE GFCI BREAKERS
<input type="checkbox"/> INTERRUPTING RATING (AIC SYM)				<input type="checkbox"/> SERVICE ENTRANCE RATED		<input type="checkbox"/> MINGED DOOR	
<input type="checkbox"/> 10,000A	<input type="checkbox"/> 30,000A	<input type="checkbox"/> 65,000A	NEUTRAL BUS <input type="checkbox"/> BUS MATERIAL		<input type="checkbox"/> KEYED LATCH		NOTES ① SHUNT TRIP MAIN BREAKER FOR E-STOP CIRCUIT ② ③ ④ ⑤
<input type="checkbox"/> 14,000A	<input type="checkbox"/> 42,000A	<input type="checkbox"/> 100,000A	<input type="checkbox"/> 50% <input type="checkbox"/> 200% <input type="checkbox"/> COPPER		<input type="checkbox"/> DOOR-IN-DOOR		
<input type="checkbox"/> 22,000A	<input type="checkbox"/> 50,000A	<input type="checkbox"/> ____000A	<input type="checkbox"/> 100% <input type="checkbox"/> ____% <input type="checkbox"/> ALUMINUM		<input type="checkbox"/> STAINLESS STEEL		
<input type="checkbox"/> SERIES RATING PERMITTED			<input type="checkbox"/> GROUND BUS REQUIRED		<input type="checkbox"/> PAINTED STEEL		
<input type="checkbox"/> ____ PANELS IN PARALLEL			<input type="checkbox"/> THROUGH FEED LUGS REQUIRED		<input type="checkbox"/> NAMEPLATE		



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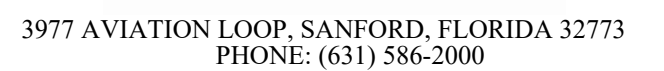
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INSTALLATION OF A NEW  
ABOVE GROUND FUEL TANK  
LEADING EDGE AVIATION  
LOGAN-CACHE AIRPORT  
2500 NORTH AIRPORT DRIVE  
LOGAN, UTAH 81321

SHEET DESCRIPTION:

## ELECTRICAL DIAGRAMS & PANELBOARD SCHEDULE

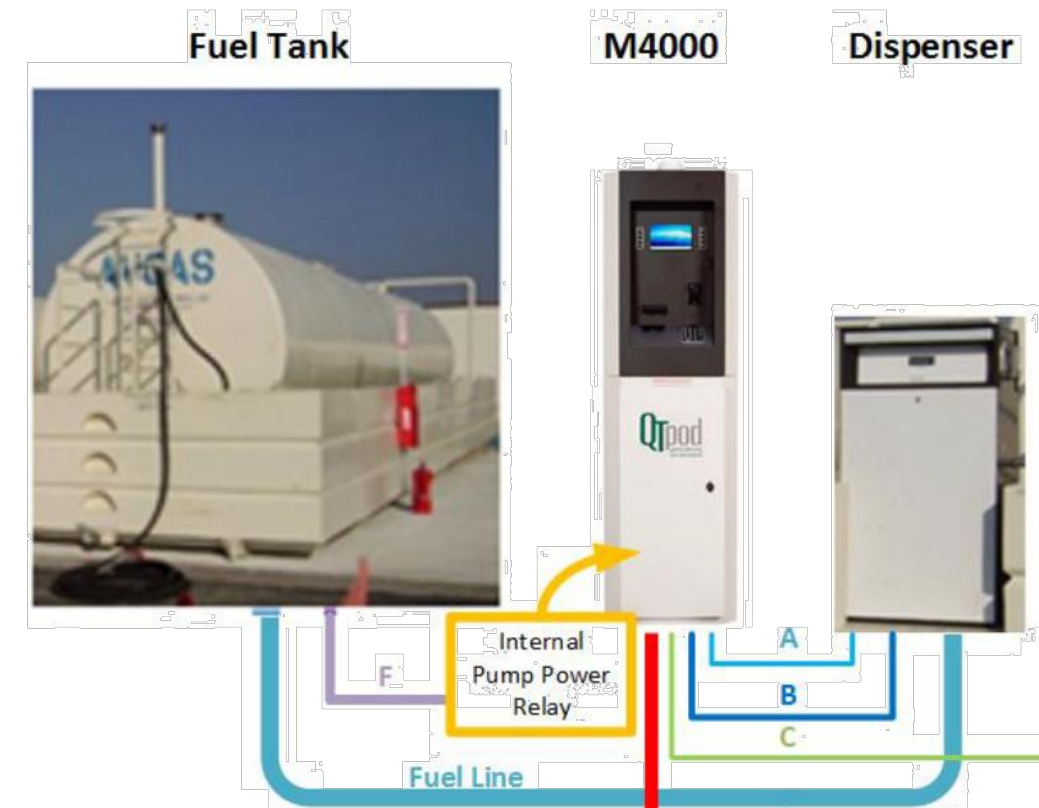
SEAL &amp; SIGNATURE

DATE: DECEMBER 2019
PROJECT NO.: AEAC-LOGAN
DRAWING BY: MSK
CHK. BY: AGN
DWG No:

E-300

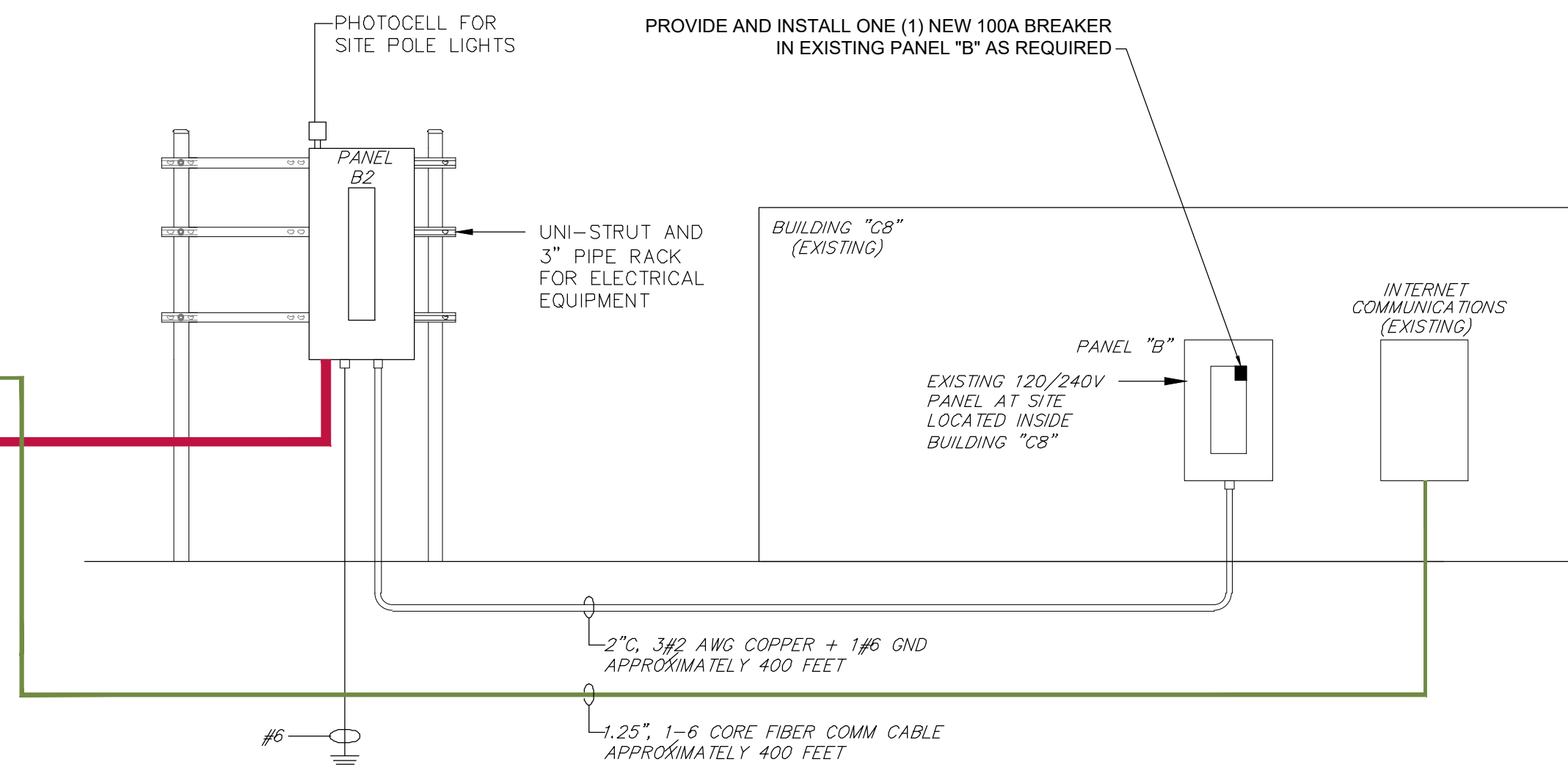
BRIAN E. LEWIS, P.E.  
UTAH P.E. # 5013586-2203  
EXP. DATE: 3/31/2021

CADD FILE NO.  
LOGAN-CACHE-Set.dwg



**Figure 8 - Dispenser Field Wiring - Option B**

<u>Rigid Conduit</u>	<u>Size</u>	<u>Description of wires inside conduit</u>
A	1/2"	Low voltage for pulser wires
B	3/4"	High voltage for dispenser signal wire (reset, clear, slow & fast valves)
C	1/2"	Low voltage for Ethernet Cable (if needed)
D	3/4"	Multiple High voltage for pump motor, terminal and dispenser power
E	1/2"	High voltage pump control wiring



SOURCE = 120/240V NO SCALE



LOGAN – CACHE AIRPORT AUTHORITY BOARD  
FEBRUARY 6, 2025

# ATTACHMENT E

TOWER REPAIR - Specifications Historic Tower Logan-Cache Airport

Clean up-Remove and dispose of all debris on all levels (four levels) of the structure

Tear out and remove carpet, pad, and damaged drywall

Tear off and remove acoustic ceiling tiles

Repair Roof

Roof-Tear off and Dispose of existing roofing materials

Install new 60 mil membrane

Install flashing and curbing

Glass Repair

Install tempered glass to match existing material in existing frame works.

Replace the rotting wood sill material

Install flashing to preserve wood sills

Paint/seal wood sills to preserve

Other

ALTERNATE – TOWER DEMO

Demolish tower and dispose of all debris

Restore site to match surrounding area

	Raymond Construction	Sorensen & Gnehm Const	Paul Davis
		3,500.00	
	\$ 4,060.00	2,400.00	3,458.95
	\$ 1,150.00	2,600.00	711.74
	\$ 8,125.00	8,430.00	1,455.32
	\$ 13,329.00		2,815.63
	\$ inc		2,435.30
	\$ 56,801.00	68,801.00	12,544.79
	\$ 3,133.00	3,200.00	987.97
	\$ 2,130.00		7,139.86
	\$ 1,737.00		582.94
		<u>18,326.20</u>	<u>-</u>
	90,465.00	107,257.20	32,132.50
	\$ 62,506.00	-	-
	\$ <u>7,887.00</u>	-	-
	70,393.00	-	-

**LOGAN – CACHE AIRPORT AUTHORITY BOARD  
MEETING PACKET  
MARCH 13, 2025**

# **AGENDA ITEM**

**2.b.**

# **LOGAN-CACHE AIRPORT BOARD AUTHORITY**

## **RESOLUTION NO. 2025-01**

### **A RESOLUTION ADOPTING LANDING FEES AT THE LOGAN-CACHE AIRPORT**

WHEREAS, A Governance and Operations Audit of the Logan-Cache Airport was conducted in 2024 by Dr. Mike Jones of Swiebar-Zhong Consultancy, and;

WHEREAS, The audit recommends the implementation of a landing fee program to generate much-needed revenue to help cover airport operation and management costs, and;

WHEREAS, The Airport Board Authority of Logan-Cache Airport, in a regular meeting, lawful notice of which has been given, held on February 6, 2025, finds that it is necessary to adopt the practice of billing landing fees for non-based aircraft.

NOW THEREFORE, BE IT RESOLVED that the Logan-Cache Airport Authority Board hereby adopts the following resolution:

The Logan-Cache Airport shall collect landing fees from aircraft that land at Logan-Cache airport. Landing fees shall be collected from aircraft which are not based at the airport, and which weigh more than 6,000 pounds.

The landing fee shall be tiered, based on the maximum gross weight of the aircraft. The first tier begins at  $\geq 6,000$  pounds, up to 14,999 pounds. This tier will be charged \$3.00 per 1,000 pounds. The second tier  $\geq 15,000$  pounds will be charged \$4.00 per 1,000 pounds.

This resolution was duly adopted by the Logan-Cache Airport Authority Board on the 13th day of March, 2025.

Logan-Cache Airport Authority

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David Zook, Airport Authority Board Chairman